

RZC 21.10 Downtown Regulations

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21.10.010 Purpose

The purposes of the Downtown Regulations are to:

- (A) Implement the Downtown vision and policies as described in the Comprehensive Plan;
- (B) Promote the development of Downtown as an Urban Center, attracting people and businesses by providing an excellent transportation system, diverse economic opportunities, a variety of well-designed and distinctive places to live, and proximity to shopping, recreation, and other amenities;
- (C) Provide a pedestrian- and bicycle-oriented environment with “local” streets appropriate for a destination location; and
- (D) Provide a dynamic urban area that is enhanced by a rich natural setting, including open space, trees, and other landscaping, and a focus on the Sammamish River. Such a neighborhood, by its very nature, is noisier and busier than the typical suburban residential neighborhood.

21.10.020 Downtown Zones Map

The Downtown neighborhood – less than one square mile in size – contains 12 zones called: Valley View, Bear Creek, Trestle, Sammamish Trail, Town Square, Old Town, Anderson Park, River Bend, River Trail, East Hill, Carter, and Town Center. These individual zones will continue to develop as distinct places within the Downtown, characterized by different building heights, designs and land uses, distinctive entrance corridors, streetscapes, roadway designs, landscaping and amenities. This variety helps draw people to and through the various zones. The districts are shown on the map entitled “Downtown Zones” below.

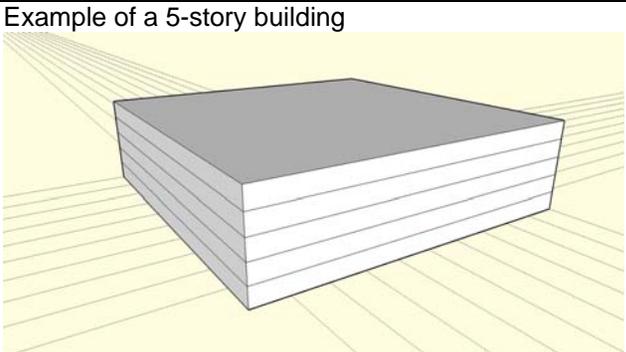
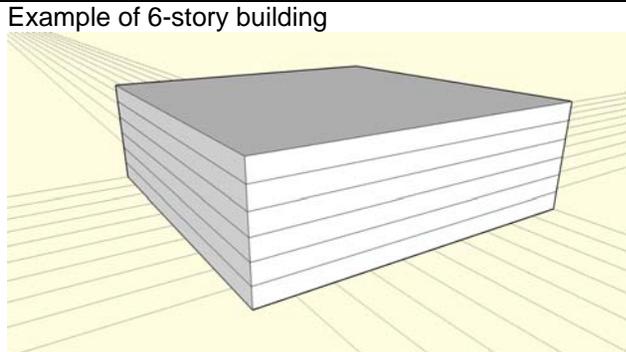
INSERT MAP 10.1

21.10.030 Old Town (OT) Zone

(A) Purpose.

The Old Town district is comprised of the original downtown and includes a number of historic structures and gathering places, including a central park. The Old Town district is established to be a center of pedestrian-oriented retail activity in the Downtown neighborhood. The regulations shaping development in this district provide for an urban village pattern and rhythm which encourages narrow ground floor storefronts, small blocks, narrow streets with curbside parking, mixed-use residential/office/service buildings, and pedestrian-scale architecture. The regulations also encourage the enhancement of existing older buildings in Old Town with small ground floor retail spaces, characterized by narrow bay spacing complementary to pedestrian activity and interest, and office or residential spaces in upper stories. The pedestrian nature of the area is emphasized through lower parking requirements and plans for creation of parking lots/structures in a few central locations at the edge of these areas. This district provides for a full range of retail uses such as specialty and comparison shopping, eating and entertainment establishments, as well as general and professional services, and residential uses.

(B) Maximum Development Yield.

Allowed	Base	Maximum	Example of a 5-story building	Example of 6-story building
Height	5 stories	6 stories		
Lot Coverage	100%	100%		

These are office building examples using Transfer Development Rights [and Green Building Program](#) to achieve the maximum achievable floor area within the maximum allowed building height. Residential and mixed-use residential developments may have similar height, but volume will differ due to setback and open space requirements.

(C) Regulations Common to All Uses.

Regulation	Standard	Notes and Exceptions
Front Setback (distance from back of curb)		
Front and side street (commercial use)	See Map 10.3 Downtown Pedestrian System Map	Setbacks along Downtown Streets are regulated by the Downtown Pedestrian System which specifies street frontage standards between the street curb and the face of buildings, depending on site location.
Front and side street (residential use on ground floor)		Not permitted on ground floor street fronts of Type I pedestrian streets (Map 10.3, Downtown Pedestrian System Map).
Setback line (distance from property line)		
Side commercial	0 feet	
Rear commercial	0 feet	
Side (residential)	See RZC 21.10.130(D) Residential Setback Requirements	
Rear (residential)	10 feet	
Yard adjoining BNSF ROW or Parks	14 feet	
Yard adjoining Mid-Block Path	See Map 10.3 Downtown Pedestrian System Map	
Other Standards		
Minimum Building Height	n/a	
Maximum Building Height w/o TDRs	5 Stories	1. Building height is limited to three stories for some parcels fronting Leary Way. See RZC 21.10.110(B) Height Limit Overlay.
Maximum Building Height w/ TDRs and GBP	6 Stories	1. Building height is limited to three stories for some parcels fronting Leary Way. See RZC 21.10.110(B) Height Limit Overlay. 2. One floor of additional height may be achieved with the use of Transfer Development rights (See RZC 21.10.160, Using Transfer Development Rights) or through compliance with the Green Building Program (See RZC 21.67, Green Building Infrastructure Program) .
Maximum Lot Coverage	100%	1. For residential development without ground floor commercial/office, lot coverage shall be governed by the Residential Density Chart, RZC 20.10.130(D).
Base FAR Without TDRs	1.25	Maximum FAR without TDRs or the GBP , for non-residential space, is 1.25. Residential space within a mixed-use building is exempt from TDR or GBP requirements. (See RZC 21.10.160, Using Transfer Development Rights) or through compliance with the Green Building Program (See RZC 21.67, Green Building Infrastructure Program) 2. All legal lots are entitled to 10,000 square feet GFA without the use of TDRs or GBP provided that other site

		requirements can be met.
Allowed Residential Density	Depends on Lot Size	See RZC 21.10.130(B), Downtown Residential Density Chart.

(D) Allowed Uses and Basic Development Standards.

The following table contains the basic zoning regulations that apply to uses. To use the chart, read down the left-hand column titled "Use." When you have located the use that interests you, read across to find regulations that apply to that use. Uses are permitted unless otherwise specified in the Special Regulations column. Permitted uses may require land use permit approval. See RZC 21.76.020, Overview of the Development Process, for more information. Uses not listed are not permitted.

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
Residential			
1	Multi-Family Structure, Mixed-Use Residential	Dwelling Unit (1.0, 2.25) Plus one guest space per four units for projects with six units or more. Curbside parking along the site may be counted towards up to 25 percent of the required off-street parking.	<ol style="list-style-type: none"> 1. Maximum density per lot dependent upon size and width of lot, per RZC 21.10.130(B), Downtown Residential Densities Chart. 2. Not permitted on ground floor street fronts of Type I pedestrian streets (Map 10.3, Downtown Pedestrian System Map), or where ground floor residences may be negatively impacted by nearby nonresidential uses. Residential uses may be allowed on ground floor streets fronts of Type II Pedestrian Streets, per RZC 21.62.020(F)(5), Ground Floor Residential Uses on Type II Pedestrian Streets, but not within the shorter distance of 100 feet or one-quarter block length from a street intersection. 3. Affordable Housing requirements apply to developments of 10 units or more. See RZC 21.20.020. 4. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
2	Dormitory	Bed (0.5, 1.0)	The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
3	SRO		

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
4	Housing Services for the Elderly	See Special Regulations	Parking requirements are as follows: <ol style="list-style-type: none"> 1. Multifamily housing for senior citizens: Unit (0.5, 2.0) 2. Nursing home or long-term care facility: 4 patient beds (1.0, 1.0) 3. Retirement residence with no skilled nursing facility: Unit (1.0, 1.0) 4. Retirement residence with skilled nursing facility: Worker on largest shift (1.25, 1.25) 5. <u>A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system.</u> 6. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>

General Sales or Service			
5	General Sales or Services	1,000 sq ft gfa (2.0, 2.0)	<ol style="list-style-type: none"> 1. Uses not permitted include: <ol style="list-style-type: none"> a. Gasoline service. b. Auto repair. c. Automobile sales with outdoor display and storage. d. Rental storage and mini-warehouses. e. Retail sales or services involving drive-through/drive-up facilities, except drive-through facilities confined within the garage of a multi-story (at least three stories) building shall be allowed when the drive-through lanes provide a queuing length adequate to serve peak demand without overflowing onto public sidewalks or streets, as determined by a professional traffic engineer and approved by the City. f. Uses that are materially detrimental to typical downtown office and residential uses in terms of excessive noise and vibration, truck traffic, fumes, and other potential impacts. 2. Parking standards for restaurant uses: <ol style="list-style-type: none"> a. Sit-down restaurant: 1,000 SF GFA (2.0, 9.0). b. Take-out restaurant: 1,000 SF GFA (2.0, 10.0). c. The Technical Committee may waive the parking requirement for restaurant/deli/café uses 1,000 square feet gfa, or less in area that support/enhance the City's vision for creating/enhancing Downtown as a pedestrian place provided: <ol style="list-style-type: none"> i. The use is located in an office building and primarily serves the occupants and guests of the office building; or ii. The use is visible from, and within 100 feet of a promenade or Downtown park, such as Luke McRedmond Park, Anderson Park, O'Leary Park, The Edge Skate Park or the 83rd Street Promenade, for example, or within 100 feet of a critical areas buffer of the Sammamish River and access to the River Trail, and the use is designed to enliven the pedestrian environment and primarily cater to pedestrians and outdoor patrons. 3. The maximum number of parking stalls allowed may be increased to 5.0 per 1,000 square feet of gross floor area for the retail components of mixed-use developments. 4. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
6	Hotels, Motels, and Other Accommodation Services	Rental room (1.0, 1.0)	<u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
Transportation, Communication, Information, and Utilities			
7	Road, Ground Passenger and Transit Transportation	1,000 sq ft gfa (2.0, 2.0)	1. Regional light rail transit system and office uses only. No vehicle storage except light rail vehicles.
8	Rapid charging station	Adequate to accommodate peak use	1. Shall not be located on a parcel that abuts a residential zone.
9	Battery exchange station		

10	Communications and Information	1,000 sq ft gfa (2.0, 2.0)	
11	Local Utilities		
12	Large Satellite Dishes/Amateur Radio Antenna(s)	Adequate to accommodate peak use	See RZC 21.56 Wireless Communications Facilities
13	Wireless Communication Facilities		
14	Broadcast and Relay Towers		1. Requires a conditional use permit. See RZC 21.76.070(K) Conditional Use Permit.
Arts, Entertainment, and Recreation			
15	Arts, Entertainment, and Recreation	Adequate to accommodate peak use	
Education, Public Administration, Health Care, and Other Institutions			
16	Education, Public Administration, Health Care, and other Institutions, except those listed below		<ol style="list-style-type: none"> 1. <u>Parking</u>: The number of spaces must be adequate to accommodate the peak customer and employee shift, demonstrated by a parking study or other study submitted by the applicant and approved by the Code Administrator. 2. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,</u>
17	Day Care Center	See Special Regulations	<ol style="list-style-type: none"> 1. Provisions for day care centers include: <ol style="list-style-type: none"> a. Shall provide parking as follows: Employee on maximum shift (1.0, 1.0). b. Play equipment shall be located no less than 10 feet from any property line. c. <u>Parking</u>: The number of spaces must be adequate to accommodate the peak shift as determined by the Code Administrator after considering the probable number of employees, etc. 2. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,</u>
18	Religious Institutions	1,000 sq ft gfa (10.0, 10.0) or 1/5 fixed seats	<ol style="list-style-type: none"> 1. A seat is one fixed seat or 18 inches on a pew or bench or seven square feet in the general assembly area (including aisle space, but excluding stage, podium, lobby, and space for musical instruments). 2. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 3. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,</u>
19	Funeral Homes and Services		<ol style="list-style-type: none"> 1. Excludes crematoriums. 2. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,</u>
Other			

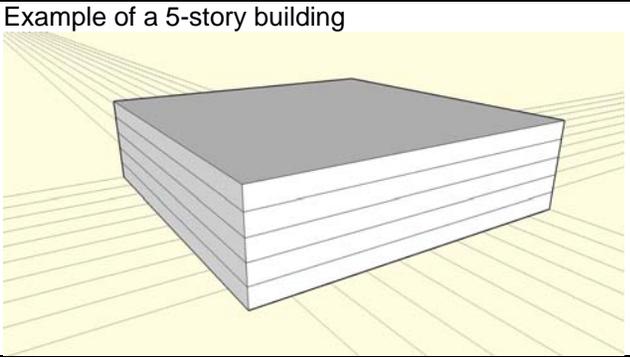
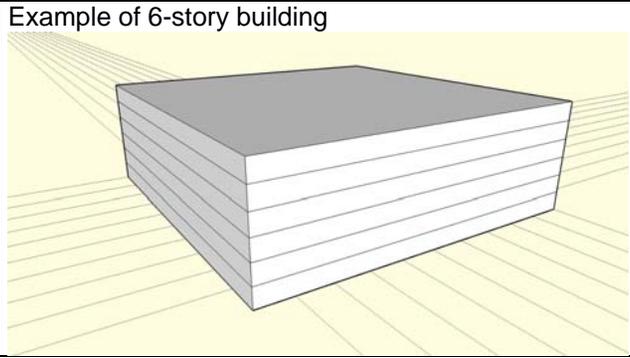
20	Vending Carts, Kiosks		<ol style="list-style-type: none"> 1. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. 2. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. 3. Structures shall be secured to prevent tipping and endangering public safety. 4. Maximum size is six feet wide by ten feet long. 5. Administrative design review required for structures.
21	Automobile Parking Facilities		<ol style="list-style-type: none"> 1. Surface parking lots are prohibited.

21.10.040 Anderson Park (AP) Zone

(A) Purpose.

The Anderson Park (AP) zone is one of four distinct mixed-use residential/office zones in the Downtown Neighborhood.; Sammamish Trail (SMT), Town Square (TSQ), Anderson Park (AP), and River Bend (RVBD). All four zones are intended to provide for significant residential growth, as well as opportunities for growth in professional, business, health, and personal services. These zones provide for a range of employment uses, such as financial and professional services, public administrative offices, health services, advanced technology industries, universities and technical colleges, and other activities normally conducted in multi-story office structures. These zones are intended as areas for the densest employment and residential uses in the Downtown, but also provide for supporting retail, service, and entertainment uses located within walking distance of each other. Some of the quieter streets in these districts allow residential uses on the ground floor, whereas the primary streets are intended for active, pedestrian-friendly and activating commercial uses. The Anderson Park zone encourages development of a mix of multi-story residential and office buildings and allows limited retail space in order to provide convenient access without diluting the more concentrated retail cores of the Old Town and Town Center zones. The Anderson Park zone also enhances the long-term pedestrian character of Redmond Way and Cleveland Street by including street-level building and landscape design.

(B) Maximum Development Yield.

Allowed	Base	Maximum	Example of a 5-story building	Example of 6-story building
Height	5 stories	6 stories		
Lot Coverage	100 percent	100 percent		
<p>These are office building examples using Transfer Development Rights or Green Building Program to achieve the maximum achievable floor area within the maximum allowed building height. Residential and mixed-use residential developments may achieve similar results.</p>				

(C) Regulations Common to All Uses.

Regulation	Standard	Notes and Exceptions
Front Setback (distance from back of curb)		
Front and side street (commercial use)	See Map 10.3 Downtown Pedestrian System Map	Setbacks along Downtown Streets are regulated by the Downtown Pedestrian System which specifies street frontage standards between the street curb and the face of buildings, depending on site location.
Front and side street (residential use on ground floor)	See RZC 21.10.130(D) Residential Setback Requirements	Not permitted on ground floor street fronts of Type I pedestrian streets (Downtown Pedestrian System Map). Residential uses may be allowed on ground floor streets fronts of Type II Pedestrian Streets per 21.62.020(F)(5), Ground Floor Residential Uses on Type II Pedestrian Streets, but not within the shorter distance of 100 feet or one-quarter block length from a street intersection.
Setback line (distance from property line)		
Side commercial	0 feet	
Rear commercial	0 feet	
Side (residential)	See RZC 21.10.130(D) Residential Setback Requirements	
Rear (residential)	10 feet	
Yard adjoining BNSF ROW or Parks	14 feet	
Yard adjoining Mid-Block Path	See Map 10.3 Downtown Pedestrian System Map	
Other Standards		
Minimum Building Height	n/a	
Maximum Building Height w/o TDRs or GBP	5 Stories	
Maximum Building Height w/ TDRs or GBP	6 Stories	1. One floor of additional height may be achieved with the use of Transfer Development rights (See RZC 21.10.160, Using Transfer Development Rights) or Green Building Program (See RZC 21.67, Green Building Infrastructure Program).
Maximum Lot Coverage	100 percent	1. For residential development without ground floor commercial/office, lot coverage shall be governed by RZC 21.10.130(B), Residential Density Chart.
Base FAR Without TDRs	1.25	1. Maximum FAR without TDRs, for nonresidential space, is 1.25. Residential space within a mixed-use building is exempt from TDR or Green Building requirements. (See RZC 21.10.160, Using Transfer Development Rights or RZC 21.67, Green Building Infrastructure Program). 2. All legal lots are entitled to 10,000 square feet GFA without the use of TDRs or GBP provided that other site requirements can be met.

Allowed Residential Density	Depends on Lot Size	See RZC 21.10.170, Downtown Residential Density Chart.
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(D) Allowed Uses and Basic Development Standards.

The following table contains the basic zoning regulations that apply to uses. To use the chart, read down the left-hand column titled "Use." When you have located the use that interests you, read across to find regulations that apply to that use. Uses are permitted unless otherwise specified in the Special Regulations column. Permitted uses may require land use permit approval. See RZC 21.76.020, Overview of the Development Process, for more information. Uses not listed are not permitted.

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
Residential			
1	Multifamily Structure, Mixed-Use Residential	Dwelling Unit (1.0, 2.25) Plus one guest space per four units for projects with six units or more. Curbside parking along the site may be counted towards up to 25 percent of the required off-street parking.	<ol style="list-style-type: none"> 1. Maximum density per lot dependent upon size and width of lot, per RZC 21.10.130(B), Downtown Residential Densities Chart. 2. Not permitted on ground floor street fronts of Type I pedestrian streets (see Map 10.3, Downtown Pedestrian System Map), or where ground floor residences may be negatively impacted by nearby nonresidential uses, except through establishment of an Administrative Design Flexibility per RZC 21.76.070(C). Residential uses may be allowed on ground floor streets fronts of Type II Pedestrian Streets per RZC 21.62.020(F)(5), Ground Floor Residential Uses on Type II Pedestrian Streets, but not within the shorter distance of 100 feet or one-quarter block length from a street intersection. 3. Affordable Housing requirements apply to developments of 10 units or more. See RZC 21.20.020. 4. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
2	Dormitory	Bed (0.5, 1.0)	The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
3	SRO		

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
4	Housing Services for the Elderly	See Special Regulations	<p>Parking requirements are as follows:</p> <ol style="list-style-type: none"> 1. Multifamily housing for senior citizens: Unit (0.5, 2.0). 2. Nursing home or long-term care facility: 4 patient beds (1.0, 1.0). 3. Retirement residence with no skilled nursing facility: Unit (1.0, 1.0). 4. Retirement residence with skilled nursing facility. Worker on largest shift (1.25, 1.25) 5. <u>A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system.</u> 6. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>

General Sales or Service			
5	General Sales and Services	1,000 sq ft gfa (2.0, 3.5)	<ol style="list-style-type: none"> 1. Uses not permitted include: <ol style="list-style-type: none"> a. Gasoline service. b. Auto repair. c. Automobile sales with outdoor display and storage. d. Rental storage and mini-warehouses. e. Retail sales or services involving drive-through/drive-up facilities, except drive through facilities confined within the garage of a multi-story (at least three stories) building shall be allowed when the drive-through lanes provide a queuing length adequate to serve peak demand without overflowing onto public sidewalks or streets, as determined by a professional traffic engineer and approved by the City. 2. Shall not be materially detrimental in terms of noise, truck traffic and other potential operational impacts with nearby multi-story mixed-use/residential developments. 3. Parking standards for restaurant uses: <ol style="list-style-type: none"> a. Sit-down restaurant: 1,000 SF GFA (9.0, 9.0). b. Take-out restaurant: 1,000 SF GFA (10.0, 10.0). d. 1,000 sq ft gfa (5.0, 5.0) when the restaurant is located in a multi-story building at least 3 stories tall. e. The Technical Committee may waive the parking requirement for restaurant/deli/café uses less than 750 square feet gfa that support/enhance the City's vision for creating/enhancing Downtown as a pedestrian place provided : <ol style="list-style-type: none"> i. The use is located in an office building and primarily serves the occupants and guests of the office building; or ii. The use is visible from, and within 100 feet of a promenade or Downtown park, such as Luke McRedmond Park, Anderson Park, O'Leary Park, The Edge Skate Park or the 83rd Street Promenade, for example, or within 100 feet of a critical areas buffer of the Sammamish River and access to the River Trail, and the use is designed to enliven the pedestrian environment and primarily cater to pedestrians and outdoor patrons. 4. The maximum number of parking stalls allowed may be increased to 5.0 per 1,000 square feet of gross floor area for the retail components of mixed-use developments. 5. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
6	Hotels, Motels, and Other Accommodation Services	Rental room (1.0, 1.0)	<u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
Transportation, Communication, Information, and Utilities			

7	Road, Ground Passenger and Transit Transportation	1,000 sq ft gfa (2.0, 3.5)	1. Regional light rail transit system and office uses only. No vehicle storage.
8	Rapid charging station	Adequate to accommodate peak use	1. Shall not be located on a parcel that abuts a residential zone.
9	Battery exchange station		
10	Communications and Information	1,000 sq ft gfa (2.0, 3.5)	
11	Local Utilities		
12	Large Satellite Dishes/Amateur Radio Antenna(s)		
13	Wireless Communication Facilities		1. Requires a conditional use permit. See RZC 21.76.070(K) Conditional Use Permit.
14	Broadcast and Relay Towers		
Arts, Entertainment, and Recreation			
15	Arts, Entertainment, and Recreation	Adequate to accommodate peak use	
Education, Public Administration, Health Care, and other Institutions			
16	Education, Public Administration, Health Care, and other Institutions, except those listed below	See Special Regulations	<ol style="list-style-type: none"> 1. Parking: The number of spaces must be adequate to accommodate the peak customer and employee shift, demonstrated by a parking study or other study submitted by the applicant and approved by the Code Administrator. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
17	Day Care Center		<ol style="list-style-type: none"> 1. Provisions for day care centers include: <ol style="list-style-type: none"> a. Shall provide parking as follows: Employee on maximum shift (1.0, 1.0) b. Play equipment shall be located no less than 10 feet from any property line c. Parking: The number of spaces must be adequate to accommodate the peak shift as determined by the Code Administrator after considering the probable number of employees, etc. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
18	Religious Institutions		1,000 sq ft gfa (10.0, 10.0) or 1/5 fixed seats

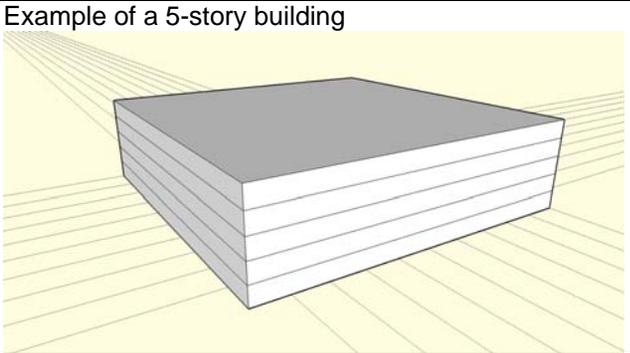
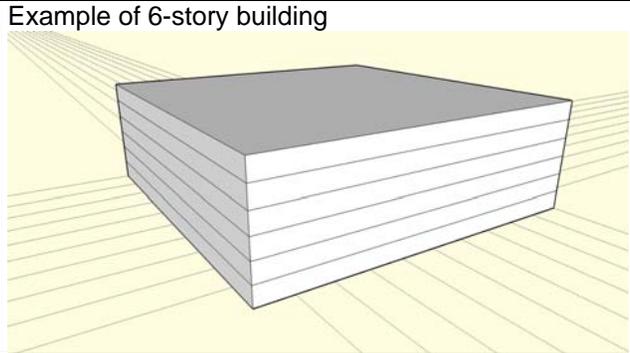
19	Funeral Homes and Services		<ol style="list-style-type: none"> 1. Excludes crematoriums. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,
Other			
20	Vending Carts, Kiosks		<ol style="list-style-type: none"> 1. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. 2. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. 3. Structures shall be secured to prevent tipping and endangering public safety. 4. Maximum size is six feet wide by ten feet long. 5. Administrative design review required for structures.
21	Automobile Parking Facilities		<ol style="list-style-type: none"> 1. Surface parking lots are prohibited.

21.10.050 Town Center (TWNC) Zone

(A) Purpose.

Town Center is one of the City's primary gathering places. Its mix of shops and restaurants, offices, hotel rooms and conference facilities, and eventually residences in the heart of the City is intended to bring people together during the day and evening for planned or casual meetings. The design of the buildings, street patterns, and public plazas are modern yet reflect the historic district in adjacent Old Town. Improvements in walking connections between the two districts will help both areas thrive. The long-term vision for Town Center is that it will continue to develop as a major gathering and entertainment place within the community, that its trails will be connected to Marymoor Park by a grade-separated connection across SR 520, and that transit service to and from the center will provide a choice equal in attractiveness to automobiles, walking, and biking. The design and development of this zone is controlled by a Master Plan established to ensure that development here integrates with and positively influences future redevelopment of the greater downtown area, and retains traditional building styles, street patterns, variety of uses, and public amenities.

(B) Maximum Development Yield.

Allowed	Base	Maximum	Example of a 5-story building	Example of 6-story building
Height	5 stories	6 stories		
Lot Coverage	100 percent	100 percent		
<p>These are office building examples using Transfer Development Rights or Green Building Program to achieve the maximum achievable floor area within the maximum allowed building height. Residential and mixed-use residential developments may achieve similar results. Residential and mixed-use residential developments may have similar height, but volume will differ due to setback and open space requirements.</p>				

(C) Regulations Common to All Uses.

Regulation	Standard	Notes and Exceptions
Front Setback (distance from back of curb)		
Front and side street (commercial use)	See RZC 21.10.150. Map 10.4. Town Center Pedestrian System Map	1. Setbacks along Downtown Streets are regulated by the Downtown Pedestrian System which specifies street frontage standards between the street curb and the face of buildings, depending on site location. 2. All new development shall comply with the adopted Town Center Master Plan and Design Guidelines.
Setback line (distance from property line)		
Side commercial	0 feet	1. All new development shall comply with the adopted Town Center Master Plan and Design Guidelines.
Rear commercial	0 feet	1. All new development shall comply with the adopted Town Center Master Plan and Design Guidelines.
Side (residential)	See RZC 21.10.130(D) Residential Setback Requirements	1. All new development shall comply with the adopted Town Center Master Plan and Design Guidelines.
Rear (residential)	10 feet	1. All new development shall comply with the adopted Town Center Master Plan and Design Guidelines.
Yard adjoining BNSF ROW or Parks	14 feet	
Other Standards		
Minimum Building Height	n/a	
Maximum Building Height w/o TDRs or GBP	Varies	1. Mixed-Use area: four stories; hotel – six or eight stories with meeting, conference and banquet facilities. Office Park area: five stories. Bear Creek Retail Area: three stories. Mixed-use residential or residential use in Town Center: five stories outright. The Technical Committee shall administratively allow the height in the Mixed-Use overlay area to be increased to six stories if the building facade is recessed above the second floor and building modulation is provided to mitigate the bulk and mass from the additional height allowance.
Maximum Building Height w/ TDRs or GBP	Varies	1. One floor of additional height may be achieved with the use of Transfer Development rights (See RZC 21.10.160, Using Transfer Development Rights) or through compliance with the Green Building Program (See RZC 21.67, Green Building Infrastructure Program) except they may not be used to exceed eight stories where eight stories is allowed through bonus provisions.
Maximum Height Within Shorelines (SMP)	35 feet	1. This height limit is restricted to that portion of the building physically located within the shoreline jurisdiction. (SMP)

		2. The maximum height of structures, including bridges, that support a regional light rail transit system may be higher than 35 feet, but shall be no higher than is reasonably necessary to address the engineering, operational, environmental and regulatory issues at the location of the structure. (SMP)
Maximum Lot Coverage	Varies	1. Governed by the Downtown Element of the Comprehensive Plan and the Town Center Master Plan and Design Guidelines.
Base FAR Without TDRs	Varies	1. The maximum gross leasable area of allowed commercial space without TDRs is 1.49 million square feet. The 1.49 million square feet limit may be increased to a maximum of 1.80 million square feet through the acquisition and use of TDRs or the GBP provided that TDRs or the GBP may not be used to increase the height of the full service hotel/conference center above eight stories/100 feet, and that a minimum of 140,000 square feet be reserved for a full service hotel/conference center with banquet and meeting facilities to accommodate groups of at least 300 people. The additional square footage allowed may be used for infill retail and general service uses that are part of mixed-use residential developments or infill developments. Floor area for residential uses is exempt from TDR requirements and maximum commercial floor area limitations.
Allowed Residential Density	Depends on Lot Size	See RZC 21.10.130(B), Downtown Residential Density Chart.

(D) Allowed Uses and Basic Development Standards.

The following table contains the basic zoning regulations that apply to uses. To use the chart, read down the left-hand column titled “Use.” When you have located the use that interests you, read across to find regulations that apply to that use. Uses are permitted unless otherwise specified in the Special Regulations column. Permitted uses may require land use permit approval. See RZC 21.76.020, Overview of the Development Process, for more information. Uses not listed are not permitted.

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
Residential			
1	Multifamily Structure, Mixed-Use Residential	Dwelling Unit (1.0, 2.25) Plus one guest space per four units for projects with six units or more. Curbside parking along the site may be counted towards up to 25 percent of the required off-street parking.	<ol style="list-style-type: none"> 1. Maximum density per lot dependent upon size and width of lot, per RZC 21.10.130(B), Downtown Residential Densities Chart. 2. Affordable Housing requirements apply to developments of 10 units or more. See RZC 21.20.020. 3. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
2	Dormitory	Bed (0.5, 1.0)	The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
3	SRO		

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
4	Housing Services for the Elderly	See Special Regulations.	<p>Parking requirements are as follows:</p> <ol style="list-style-type: none"> 1. Multifamily housing for senior citizens: Unit (0.5, 2.0). 2. Nursing home or long-term care facility: 4 patient beds (1.0, 1.0). 3. Retirement residence with no skilled nursing facility: Unit (1.0, 1.0). 4. Retirement residence with skilled nursing facility: Worker on largest shift (1.25, 1.25). 5. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 6. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
General Sales or Service			

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
5	General Sales or Services	1,000 sq ft gfa (3.5, 5.0)	<ol style="list-style-type: none"> 1. Uses not permitted include: <ol style="list-style-type: none"> a. Gasoline service. b. Automobile sales with outdoor display and storage. c. Rental storage and mini-warehouses. d. Retail sales or services involving drive-through/drive-up facilities, except drive-through facilities confined within the garage of a multi-story (at least three stories) building shall be allowed when the drive-through lanes provide a queuing length adequate to serve peak demand without overflowing onto public sidewalks or streets, as determined by a professional traffic engineer and approved by the City. 2. Shall not be materially detrimental in terms of noise, truck traffic and other potential operational impacts with nearby multi-story mixed-use/residential developments. 3. Auto repair without outdoor storage and outdoor service is allowed provided: <ol style="list-style-type: none"> a. All service/repair work is performed indoors. b. There is no overnight storage of customer vehicles in outdoor parking areas. 4. Parking standards for restaurant uses: <ol style="list-style-type: none"> a. Sit-down restaurant: 1,000 SF GFA (9.0, 9.0). b. Take-out restaurant: 1,000 SF GFA (10.0, 10.0). c. The Technical Committee may waive the parking requirement for restaurant/deli/café uses less than 750 square feet gfa that support/enhance the City's vision for creating /enhancing Downtown as a pedestrian place provided : <ol style="list-style-type: none"> i. The use is located in an office building and primarily serves the occupants and guests of the office building; or ii. The use is visible from, and within 100 feet of a promenade or Downtown park, such as Luke McRedmond Park, Anderson Park, O'Leary Park, The Edge Skate Park or the 83rd Street Promenade, for example, or within 100 feet of a critical areas buffer of the Sammamish River and access to the River Trail, and the use is designed to enliven the pedestrian environment and primarily cater to pedestrians and outdoor patrons. 5. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,</u>
6	Hotels, Motels, and Other Accommodation Services	Rental room (1.0, 1.0)	

Transportation, Communication, Information, and Utilities

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
7	Road, Ground Passenger and Transit Transportation	1,000 sq ft gfa (3.5, 5.0)	1. Regional light rail transit system and office uses only. No vehicle storage.
8	Rapid charging station	Adequate to accommodate peak use	1. Shall not be located on a parcel that abuts a residential zone.
9	Battery exchange station		
10	Communications and Information	1,000 sq ft gfa (3.5, 5.0)	
	Local Utilities		
11	Large Satellite Dishes/Amateur Radio Antenna(s)	Adequate to accommodate peak use	See RZC 21.56 Wireless Communications Facilities.
12	Wireless Communication Facilities		1. Requires a conditional use permit. See RZC 21.76.070(K) Conditional Use Permit.
13	Broadcast and Relay Towers		
Arts, Entertainment, and Recreation			
14	Arts, Entertainment, and Recreation	Adequate to accommodate peak use	
Education, Public Administration, Health Care, and other Institutions			
15	Education, Public Administration, Health Care, and other Institutions, except those listed below	See Special Regulations	<p>1. Parking: The number of spaces must be adequate to accommodate the peak customer and employee shift, demonstrated by a parking study or other study submitted by the applicant and approved by the Code Administrator.</p> <p>2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</p>
16	Day Care Center		<p>1. Provisions for day care centers include:</p> <p>a. Shall provide parking as follows: Employee on maximum shift (1.0, 1.0).</p> <p>b. Play equipment shall be located no less than 10 feet from any property line.</p> <p>c. Parking: The number of spaces must be adequate to accommodate the peak shift as determined by the Code Administrator after considering the probable number of employees, etc.</p> <p>2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</p>

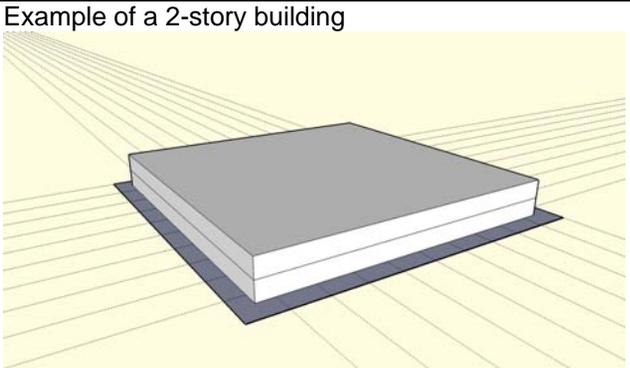
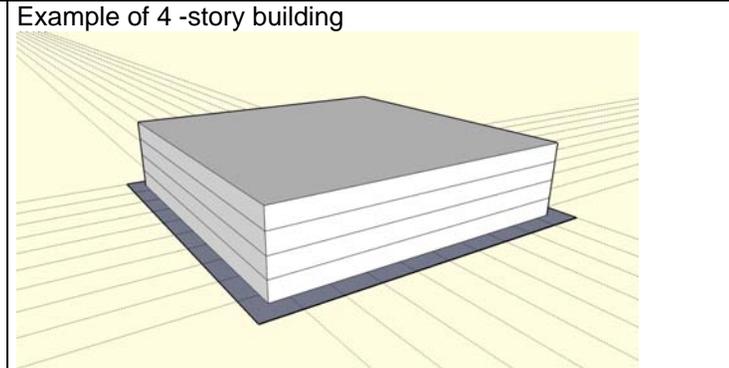
Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
17	Religious Institutions	1,000 sq ft gfa (10.0, 10.0) or 1/5 fixed seats	<ol style="list-style-type: none"> 1. A seat is one fixed seat or 18 inches on a pew or bench or seven square feet in the general assembly area (including aisle space, but excluding stage, podium, lobby, and space for musical instruments). 2. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 3. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
Other			
18	Funeral Homes and Services	1,000 sq ft gfa (10.0, 10.0) or 1/5 fixed seats	<ol style="list-style-type: none"> 1. Excludes crematoriums. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
19	Vending Carts, Kiosks		<ol style="list-style-type: none"> 1. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. 2. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. 3. Structures shall be secured to prevent tipping and endangering public safety. 4. Maximum size is six feet wide by ten feet long. 5. Administrative design review required for structures.
20	Automobile Parking Facilities		<ol style="list-style-type: none"> 1. Surface parking lots are prohibited.

21.10.060 Bear Creek, Valley View, and Trestle Zones

(A) Purpose.

The convenience commercial areas of Downtown include the Valley View, Trestle, and Bear Creek zones. These three zones are located at the major entrances to the Downtown to serve shoppers conveniently from both within and outside the neighborhood. These zones are intended to provide for everyday, basic shopping needs and services, such as groceries, pharmacies, and other convenience retail goods and services that are easily accessed by pedestrians, bicyclists, and motor vehicles. Land uses and redevelopment in the area should be compatible with shopping and service needs of the community and surroundings, as well as with the long-term Downtown vision of encouraging a more pedestrian-supportive, mixed-use environment in these zones.

(B) Maximum Development Yield.

Allowed	Base	Maximum	Example of a 2-story building	Example of 4 -story building
Height	2 stories	4 stories		
Lot Coverage	80 percent	80 percent		
<p>These are office building examples using Transfer Development Rights or GBP to achieve the maximum achievable floor area within the maximum allowed building height. Residential and mixed-use residential developments may have similar height, but volume will differ due to setback and open space requirements.</p>				

(C) Regulations Common to All Uses.

Regulation	Standard	Notes and Exceptions
Front Setback (distance from back of curb)		
Front and side street (commercial use)	See Map 10.3 Downtown Pedestrian System Map.	Setbacks along Downtown Streets are regulated by the Downtown Pedestrian System which specifies street frontage standards between the street curb and the face of buildings, depending on site location.
Front and side street (residential use on ground floor)	See Map 10.3 Downtown Pedestrian System Map.	Not permitted on ground floor street fronts of Type I pedestrian streets (Downtown Pedestrian System Map). Residential uses may be allowed on ground floor streets fronts of Type II Pedestrian Streets per 21.62.020(F)(5), Ground Floor Residential Uses on Type II Pedestrian Streets, but not within the shorter distance of 100 feet or one-quarter block length from a street intersection.
Setback line (distance from property line)		
Side commercial	0 feet	
Rear commercial	0 feet	
Side (residential)	See RZC 21.10.130(D) Residential Setback Requirements	
Rear (residential)	10 feet	
Yard adjoining BNSF ROW or Parks	14 feet	
Yard adjoining Mid-Block Path	See Map 10.3 Downtown Pedestrian System Map.	
Other Standards		
Minimum Building Height	n/a	
Maximum Building Height w/o TDRs or GBP	2 Stories	1. The maximum height may be increased to four stories when the site's development plan can demonstrate the goals and objectives outlined in Comprehensive Plan Policy DT-49, and the design guidelines outlined in RZC 21.62.020(c) are met through the site plan entitlement process.
Maximum Building Height w/ TDRs or GBP	3 Stories	1. One floor of additional height may be achieved with the use of Transfer Development rights (See RZC 21.10.160, Using Transfer Development Rights) or through compliance with the Green Building Program (See RZC 21.67, Green Building Infrastructure Program) .
Maximum Height Within Shorelines (SMP)	35 feet	1. This height limit is restricted to that portion of the building physically located within the shoreline jurisdiction. (SMP) 2. The maximum height of structures, including bridges, that support a

		regional light rail transit system may be higher than 35 feet but shall be no higher than is reasonably necessary to address the engineering, operational, environmental and regulatory issues at the location of the structure. (SMP)
Maximum Lot Coverage	80 percent	<ol style="list-style-type: none"> 1. For residential development without ground floor commercial/office, lot coverage shall be governed by the Residential Density Chart. 2. Lot coverage percentage equals: The total site area measured to the property line, less pedestrian systems measured to the curb line, on-site sidewalks, landscaping, and plazas, divided by the site area measured to the curb line.
Base FAR Without TDRs	1.25	<ol style="list-style-type: none"> 1. Maximum FAR without TDRs <u>or GBP</u>, for nonresidential space, is 1.25. Residential space within a mixed-use building is exempt from TDR <u>or GBP</u> requirements. See RZC 21.10.160 Using Transfer Development rights <u>and RZC 21.67, Green Building Infrastructure Program</u> 2. All legal lots are entitled to 10,000 square feet GFA without the use of TDRs <u>or GBP</u> provided that other site requirements can be met.
Allowed Residential Density	Depends on Lot Size	See RZC 21.10.130(B), Downtown Residential Density Chart.

(D) Allowed Uses and Basic Development Standards.

The following table contains the basic zoning regulations that apply to uses. To use the chart, read down the left-hand column titled “Use.” When you have located the use that interests you, read across to find regulations that apply to that use. Uses are permitted unless otherwise specified in the Special Regulations column. Permitted uses may require land use permit approval. See RZC 21.76.020, Overview of the Development Process for more information. Uses not listed are not permitted.

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
Residential			
1	Multifamily Structure, Mixed-Use Residential	Dwelling Unit (1.0, 2.25) Plus one guest space per four units for projects with six units or more. Curbside parking along the site may be counted towards up to 25 percent of the required off-street parking.	<ol style="list-style-type: none"> 1. Maximum density per lot dependent upon size and width of lot, per RZC 21.10.130(B), Downtown Residential Densities Chart. Not permitted on ground floor street fronts of Type I pedestrian streets (see Map 10.3, Downtown Pedestrian System Map), or where ground floor residences may be negatively impacted by nearby nonresidential uses, except through establishment of an Administrative Design Flexibility per RZC 21.76.070(C). Residential uses may be allowed on ground floor streets fronts of Type II Pedestrian Streets per RZC 21.62.020(F)(5),, Ground Floor Residential Uses on Type II Pedestrian Streets, but not within the shorter distance of 100 feet or one-quarter block-length from a street intersection. 2. Affordable Housing requirements apply to developments of 10 units or more. See RZC 21.20.020. 3. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
2	Dormitory	Bed (0.5, 1.0)	The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
3	SRO		

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
4	Housing Services for the Elderly	See Special Regulations	<p>Parking requirements are as follows:</p> <ol style="list-style-type: none"> 1. Multifamily housing for senior citizens: Unit (0.5, 2.0). 2. Nursing home or long-term care facility: 4 patient beds (1.0, 1.0). 3. Retirement residence with no skilled nursing facility: Unit (1.0, 1.0). 4. Retirement residence with skilled nursing facility: Worker on largest shift (1.25, 1.25). 5. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 6. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>

General Sales or Service			
5	General Sales or Service	1,000 sq ft gfa (3.5, 5.5)	<ol style="list-style-type: none"> 1. Uses not permitted include: <ol style="list-style-type: none"> a. Automobile sales with outdoor display and storage. b. Major Auto Repair. c. Rental storage and mini-warehouses. 2. Shall not be materially detrimental in terms of noise, truck traffic and other potential operational impacts with nearby multi-story mixed-use/residential developments. 3. Auto repair without outdoor storage and outdoor service is allowed provided: <ol style="list-style-type: none"> a. All service/repair work is performed indoors. b. There is no overnight storage of customer vehicles in outdoor parking areas. 4. Parking standards for restaurant uses: <ol style="list-style-type: none"> a. Sit-down restaurant: 1,000 SF GFA (9.0, 9.0). b. Take-out restaurant: 1,000 SF GFA (10.0, 10.0). c. 1,000 sq ft gfa (5.0, 5.0) when the restaurant is located in a multi-story building at least 3 stories tall. d. The Technical Committee may waive the parking requirement for restaurant/deli/café uses less than 750 square feet gfa that support/enhance the City's vision for creating/enhancing Downtown as a pedestrian place provided : <ol style="list-style-type: none"> i. The use is located in an office building and primarily serves the occupants and guests of the office building; or ii. The use is visible from, and within 100 feet of a promenade or Downtown park, such as Luke McRedmond Park, Anderson Park, O'Leary Park, The Edge Skate Park or the 83rd Street Promenade, for example, or within 100 feet of a critical areas buffer of the Sammamish River and access to the River Trail, and the use is designed to enliven the pedestrian environment and primarily cater to pedestrians and outdoor patrons. 5. Drive-Through facilities shall have a minimum queuing distance of 120 feet which is not within the public right-of-way or on-site circulation aisles. A landscape buffer between the drive-through lane and the street shall be provided. 6. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
6	Hotels, Motels, and Other Accommodation Services	Rental room (1.0, 1.0)	<p><u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u></p>
Transportation, Communication, Information, and Utilities			

7	Road, Ground Passenger and Transit Transportation	1,000 sq ft gfa (3.5, 5.5)	1. Valley View Zone: office uses only. No vehicle storage. 2. Bear Creek and Trestle zones: Regional light rail transit system and office uses only. No vehicle storage except light rail vehicles in Trestle zone.
8	Rapid charging station	Adequate to accommodate peak use	1. Shall not be located on a parcel that abuts a residential zone.
9	Battery exchange station		
10	Communications and Information	1,000 sq ft gfa (3.5, 5.5)	
11	Local Utilities		
12	Large Satellite Dishes/Amateur Radio Antenna(s)	Adequate to accommodate peak use	See RZC 21.56 Wireless Communications Facilities.
13	Wireless Communication Facilities		1. Requires a conditional use permit. See RZC 21.76.070(K) Conditional Use Permit.
14	Broadcast and Relay Towers		
15	Arts, Entertainment, and Recreation	Adequate to accommodate peak use	
Arts, Entertainment, and Recreation			
16	Education, Public Administration, Health Care, and other Institutions, except those listed below	See Special Regulations.	1. <u>Parking</u> : The number of spaces must be adequate to accommodate the peak customer and employee shift, demonstrated by a parking study or other study submitted by the applicant and approved by the Code Administrator. 2. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,</u>
Education, Public Administration, Health Care, and other Institutions			
17	Day Care Center	See Special Regulations.	1. Provisions for day care centers include: a. Shall provide parking as follows: Employee on maximum shift (1.0, 1.0) b. Play equipment shall be located no less than 10 feet from any property line c. <u>Parking</u> : The number of spaces must be adequate to accommodate the peak shift as determined by the Code Administrator after considering the probable number of employees, etc. 2. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,</u>

18	Religious Institutions	1,000 sq ft gfa (10.0, 10.0) or 1/5 fixed seats	<ol style="list-style-type: none"> 1. A seat is one fixed seat or 18 inches on a pew or bench or seven square feet in the general assembly area (including aisle space, but excluding stage, podium, lobby, and space for musical instruments). 2. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 3. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
19	Funeral Homes and Services		<ol style="list-style-type: none"> 1. Excludes crematoriums. 2. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
Other			
20	Vending Carts, Kiosks		<ol style="list-style-type: none"> 1. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. 2. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. 3. Structures shall be secured to prevent tipping and endangering public safety. 4. Maximum size is six feet wide by ten feet long. 5. Administrative design review required for structures.
21	Automobile Parking Facilities		<ol style="list-style-type: none"> 1. Surface Parking lots are prohibited.

21.10.070 Sammamish Trail (SMT) Zone

(A) Purpose.

The Sammamish Trail (SMT) zone is one of four distinct mixed-use residential/office zones in the Downtown Neighborhood: Sammamish Trail (SMT), Town Square (TSQ), Anderson Park (AP), and River Bend (RVBD). All four zones are intended to provide for significant residential growth, as well as opportunities for growth in professional, business, health, and personal services. These zones provide for a range of employment uses, such as financial and professional services, public administrative offices, health services, advanced technology industries, universities and technical colleges, and other activities normally conducted in multi-story office structures. These zones are intended as areas for the densest employment and residential uses in the Downtown, but also provide for supporting retail, service, and entertainment uses located within walking distance of each other. Some of the quieter streets in these zones allow residential uses on the ground floor, whereas the primary streets are intended for active, pedestrian-friendly and activating commercial uses. The Sammamish Trail (SMT) zone encourages development, including restaurants and retail uses that focuses on, celebrates, and enhances the environment of the Sammamish River by providing amenities that are connected to the river, by orienting buildings to the river trail, by providing for building heights that are lower as they approach the river and higher beyond the shoreline/critical area boundaries, and by enhancing degraded shorelines adjacent to new development.

(B) Maximum Development Yield.

	Base	Maximum	Example of a 5-story building	Example of 6-story building
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Allowed			
Height	5 stories	6 stories	
Lot Coverage	100%	100%	

These are office building examples using Transfer Development Rights [or GBP](#) to achieve the maximum achievable floor area within the maximum allowed building height. Residential and mixed-use residential developments may have similar height, but volume will differ due to setback and open space requirements.

(C) Regulations Common to All Uses.

Regulation	Standard	Notes and Exceptions
Front Setback (distance from back of curb)		
Front and side street (commercial use)	See Map 10.3 Downtown Pedestrian System Map.	Setbacks along Downtown Streets are regulated by the Downtown Pedestrian System which specifies street frontage standards between the street curb and the face of buildings, depending on site location.
Front and side street (residential use on ground floor)	See RZC 21.10.130(D) Residential Setback Requirements	Not permitted on ground floor street fronts of Type I pedestrian streets (Downtown Pedestrian System Map). Residential uses may be allowed on ground floor streets fronts of Type II Pedestrian Streets per 21.62.020(F)(5), Ground Floor Residential Uses on Type II Pedestrian Streets, but not within the shorter distance of 100 feet or one-quarter block length from a street intersection.
Setback line (distance from property line)		
Side commercial	0 feet	
Rear commercial	0 feet	
Side (residential)	See RZC 21.10.130(D) Residential Setback Requirements	
Rear (residential)	10 feet	
Yard adjoining BNSF ROW or Parks	14 feet	
Yard adjoining Mid-Block Path	See Map 10.3 Downtown Pedestrian System Map.	
Other Standards		
Minimum Building Height	2 Stories	1. Minimum height two stories, except single-story retail is allowed (through December 31, 2023) on (a) the south block of NE 83rd Street between 158th Avenue NE and 160th Avenue NE (i.e., in Lots 2 and 3 of Lot Line Revision 90-01) and (b) the portions of Lot 6 of the Redmond Center Plat lying west of 158th Avenue NE if extended south. RZC 21.62.020(G) (rather than RZC 21.62.020(H)) shall apply to single-story retail development within those two areas except that RZC 21.62.020(G)(2)(a)(ii) shall not apply to the above-referenced portions of Lot 6 of the Redmond Center Plat and except that RZC 21.62.020(G)(2)(a)(i) shall not apply to the above-referenced portion of the south block of NE 83rd Street.
Maximum Building Height w/o TDRs or GBP	5 Stories	

Maximum Building Height w/ TDRs or GBP	6 Stories	1. One floor of additional height may be achieved with the use of Transfer Development rights. (See RZC 21.10.160, Using Transfer Development Rights) or through compliance with the Green Building Program (See RZC 21.67, Green Building Infrastructure Program)
Maximum Height Within Shorelines (SMP)	35 feet	<ol style="list-style-type: none"> 1. This height limit is restricted to that portion of the building physically located within the shoreline jurisdiction. (SMP) 2. The maximum height of structures, including bridges, that support a regional light rail transit system may be higher than 35 feet but shall be no higher than is reasonably necessary to address the engineering, operational, environmental and regulatory issues at the location of the structure. (SMP)
Maximum Lot Coverage	100%	<ol style="list-style-type: none"> 1. For residential development without ground floor commercial/office, lot coverage shall be governed by RZC 21.10.130(B), Residential Density Chart.
Base FAR Without TDRs or GBP	1.25	<ol style="list-style-type: none"> 1. Maximum FAR without TDRs or GBP, for non-residential space, is 1.25. Residential space within a mixed-use building is exempt from TDR or GBP requirements. See RZC 21.10.160, Using Transfer Development Rights or RZC 21.67, Green Building Infrastructure Program 2. All legal lots are entitled to 10,000 square feet GFA without the use of TDRs provided that other site requirements can be met.
Allowed Residential Density	Depends on Lot Size	See RZC 21.10.130(B), Downtown Residential Density Chart.

(D) Allowed Uses and Basic Development Standards.

The following table contains the basic zoning regulations that apply to uses. To use the chart, read down the left-hand column titled "Use." When you have located the use that interests you, read across to find regulations that apply to that use. Uses are permitted unless otherwise specified in the Special Regulations column. Permitted uses may require land use permit approval. See RZC 21.76.020, Overview of the Development Process, for more information. Uses not listed are not permitted.

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
Residential			
1	Multifamily Structure, Mixed-Use Residential	Dwelling Unit (1.0, 2.25) Plus one guest space per four units for projects with six units or more. Curbside parking along the site may be counted towards up to 25 percent of the required off-street parking.	<ol style="list-style-type: none"> 1. Maximum density per lot dependent upon size and width of lot, per RZC 21.10.130(B), Downtown Residential Densities Chart. 2. Not permitted on ground floor street fronts of Type I pedestrian streets (see Map 10.3, Downtown Pedestrian System Map), or where ground floor residences may be negatively impacted by nearby nonresidential uses, except through establishment of an Administrative Design Flexibility per RZC 21.76.070(C). Residential uses may be allowed on ground floor streets fronts of Type II Pedestrian Streets per RZC 21.62.020(F)(5), Ground Floor Residential Uses on Type II Pedestrian Streets, but not within the shorter distance of 100 feet or one-quarter block-length from a street intersection. 3. Affordable Housing requirements apply to developments of 10 units or more. See RZC 21.20.020. 4. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
2	Dormitory	Bed (0.5, 1.0)	The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
3	SRO		

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
4	Housing Services for the Elderly	See Special Regulations	<p>Parking requirements are as follows:</p> <ol style="list-style-type: none"> 1. Multi-family housing for senior citizens: Unit (0.5, 2.0). 2. Nursing home or long-term care facility: 4 patient beds (1.0, 1.0). 3. Retirement residence with no skilled nursing facility: Unit (1.0, 1.0). 4. Retirement residence with skilled nursing facility: Worker on largest shift (1.25, 1.25). 5. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 6. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>

General Sales or Service			
5	General Sales or Services	1,000 sq ft gfa (2.0, 3.5)	<ol style="list-style-type: none"> 1. Uses not permitted include: <ol style="list-style-type: none"> a. Gasoline service. b. Auto repair. c. Automobile sales with outdoor display and storage. d. Rental storage and mini-warehouses. e. Retail sales or services involving drive-through/drive-up facilities, except drive-through facilities confined within the garage of a multi-story (at least three stories) building shall be allowed when the drive-through lanes provide a queuing length adequate to serve peak demand without overflowing onto public sidewalks or streets, as determined by a professional traffic engineer and is approved by the City. 2. Shall not be materially detrimental in terms of noise, truck traffic and other potential operational impacts with nearby multi-story mixed-use/residential developments. 3. Parking standards for restaurant uses: <ol style="list-style-type: none"> a. Sit-down restaurant: 1,000 SF GFA (9.0, 9.0). b. Take-out restaurant: 1,000 SF GFA (10.0, 10.0). c. 1,000 sq ft gfa (5.0, 5.0) when the restaurant is located in a multi-story building at least 3 stories tall. d. The Technical Committee may waive the parking requirement for restaurant/deli/café uses less than 750 square feet gfa that support/enhance the City's vision for creating /enhancing Downtown as a pedestrian place provided: <ol style="list-style-type: none"> i. The use is located in an office building and primarily serves the occupants and guests of the office building; or ii. The use is visible from and within 100 feet of a promenade or Downtown park, such as Luke McRedmond Park, Anderson Park, O'Leary Park, The Edge Skate Park or the 83rd Street Promenade, for example, or within 100 feet of a critical areas buffer of the Sammamish River and access to the River Trail, and the use is designed to enliven the pedestrian environment and primarily cater to pedestrians and outdoor patrons. 4. The maximum number of parking stalls allowed may be increased to 5.0 per 1,000 square feet of gross floor area for the retail components of mixed-use developments. 5. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
6	Hotels, Motels, and Other Accommodation Services	Rental room (1.0, 1.0)	<u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
Transportation, Communication, Information, and Utilities			
7	Road, Ground Passenger and Transit Transportation	1,000 sq ft gfa (2.0, 3.5)	1. Regional light rail transit system and office uses only. No vehicle storage except light rail vehicles.
8	Rapid charging station	Adequate to accommodate peak use	1. Shall not be located on a parcel that abuts a residential zone.
9	Battery exchange station		

10	Communications and Information	1,000 sq ft gfa (2.0, 3.5)	
11	Local Utilities		
12	Large Satellite Dishes/Amateur Radio Antenna(s)	Adequate to accommodate peak use	See RZC 21.56 Wireless Communications Facilities.
13	Wireless Communication Facilities		
14	Broadcast and Relay Towers		1. Requires a conditional use permit. See RZC 21.76.070(K) Conditional Use Permit.
Arts, Entertainment, and Recreation			
15	Arts, Entertainment, and Recreation	Adequate to accommodate peak use	
Education, Public Administration, Health Care, and other Institutions			
16	Educational Services; Public Administration; Public Safety; Other Government Functions; Associations, nonprofit organizations, etc.	See Special Regulations.	<ol style="list-style-type: none"> 1. Parking: The number of spaces must be adequate to accommodate the peak customer and employee shift, demonstrated by a parking study or other study submitted by the applicant and approved by the Code Administrator. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,
17	Day Care Center	See Special Regulations.	<ol style="list-style-type: none"> 1. Provisions for day care centers include: <ol style="list-style-type: none"> a. Shall provide parking as follows: Employee on maximum shift (1.0, 1.0). b. Play equipment shall be located no less than 10 feet from any property line. c. Parking: The number of spaces must be adequate to accommodate the peak shift as determined by the Code Administrator after considering the probable number of employees, etc. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,
18	Religious Institutions	1,000 sq ft gfa (10.0, 10.0) or 1/5 fixed seats	<ol style="list-style-type: none"> 1. A seat is one fixed seat or 18 inches on a pew or bench or seven square feet in the general assembly area (including aisle space, but excluding stage, podium, lobby, and space for musical instruments). 2. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 3. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,
19	Funeral Homes and Services		<ol style="list-style-type: none"> 1. Excludes crematoriums. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,
Other			

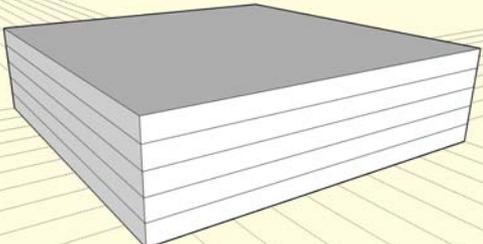
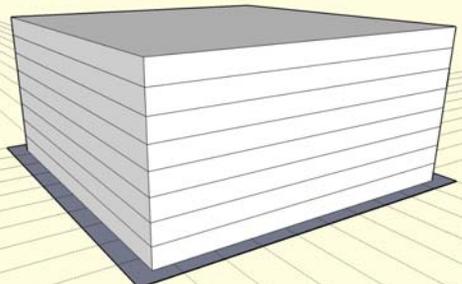
20	Vending Carts, Kiosks		<ol style="list-style-type: none"> 1. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. 2. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. 3. Structures shall be secured to prevent tipping and endangering public safety. 4. Maximum size is six feet wide by ten feet long. 5. Administrative design review required for structures.
21	Automobile Facilities	Parking	<ol style="list-style-type: none"> 1. Surface parking lots are prohibited.

21.10.080 Town Square (TSQ) Zone

(A) Purpose.

The Town Square (TSQ) zone is one of four distinct mixed-use residential/office zones in the Downtown Neighborhood: Sammamish Trail (SMT), Town Square (TSQ), Anderson Park (AP), and River Bend (RVBD). All four zones are intended to provide for significant residential growth, as well as opportunities for growth in professional, business, health, and personal services. These zones provide for a range of employment uses such as financial and professional services, public administrative offices, health services, advanced technology industries, universities and technical colleges, and other activities normally conducted in multi-story office structures. These zones are intended as areas for the densest employment and residential uses in the Downtown, but also provide for supporting retail, service, and entertainment uses located within walking distance of each other. Some of the quieter streets in these zones allow residential uses on the ground floor, whereas the primary streets are intended for active, pedestrian-friendly and activating commercial uses. The Town Square zone encourages new transit-oriented development in order to take advantage of the zones proximity to local and regional transit opportunities.

(B) Maximum Development Yield.

Allowed	Base	Maximum	Example of a 5-story building	Example of 8-story building
Height	5 stories	8 stories		
Lot Coverage	100 percent	80 percent	<p>These are office building examples using Transfer Development Rights or GBP to achieve the maximum achievable floor area within the maximum allowed building height. Residential and mixed-use residential developments may achieve similar results.</p>	

(C) Regulations Common to All Uses.

Regulation	Standard	Notes and Exceptions
Front Setback (distance from back of curb)		
Front and side street (commercial use)	See Map 10.3 Downtown Pedestrian System Map.	Setbacks along Downtown Streets are regulated by the Downtown Pedestrian System which specifies street frontage standards between the street curb and the face of buildings, depending on site location.
Front and side street (residential use on ground floor)	See Map 10.3 downtown Pedestrian System Map.	Not permitted on ground floor street fronts of Type I pedestrian streets (Downtown Pedestrian System Map). Residential uses may be allowed on ground floor streets fronts of Type II Pedestrian Streets per 21.62.020(F)(5), Ground Floor Residential Uses on Type II Pedestrian Streets, but not within the shorter distance of 100 feet or one-quarter block length from a street intersection.
Setback line (distance from property line)		
Side commercial	0 feet	
Rear commercial	0 feet	
Side (residential)	See RZC 21.10.130(D) Residential Setback Requirements	
Rear (residential)	10 feet	
Yard adjoining BNSF ROW or Parks	14 feet	
Yard adjoining Mid-Block Path	See Map 10.3 Downtown Pedestrian System Map	
Other Standards		
Minimum Building Height	2 Stories	
Maximum Building Height without TDRs <u>or</u> GBP	5 Stories	1. Building height is limited in certain areas. See RZC 21.10.110(B) Height Limit Overlay.
Maximum Building Height with TDRs <u>or</u> GBP	8 Stories	1. Building height is limited in certain areas. See RZC 21.10.110(B) Height Limit Overlay. 2. One floor of additional height may be achieved with the use of Transfer of Development rights (See RZC 21.10.160, Using Transfer of Development Rights.) <u>or through compliance with the GBP (See RZC 21.67, Green Building Infrastructure Program)</u> except they may not be used to exceed eight stories where eight stories is allowed through bonus provisions. 3. Maximum height for buildings is five stories without Transfer of Development Rights (TDRs) or bonuses. Bonus to eight stories granted for provision of 20 percent on-site usable open space in the form of

		plazas/arcades with water features that are accessible to public during extended business hours, public meeting rooms, day care services, or the preservation of historic buildings or sites. The amenities shall be on the project site or within the zone in which the building is located. Such approval shall be granted through the site plan entitlement review process. TDRs or GBP may not be used to exceed the eight-story height allowed through these bonuses
Maximum Lot Coverage	100 percent	1. For residential development without ground floor commercial/office, lot coverage shall be governed by the Residential Density Chart.
Base FAR Without TDRs or GBP	1.25	1. Maximum FAR without TDRs or GBP , for nonresidential space, is 1.25. Residential space within a mixed-use building is exempt from TDR and GBP requirements. See RZC 21.10.160, Using Transfer of Development Rights and RZC 21.67, Green Building Infrastructure Program . 2. All legal lots are entitled to 10,000 square feet GFA without the use of TDRs or GBP provided that other site requirements can be met.
Allowed Residential Density	Depends on Lot Size	See RZC 21.10.130(B), Downtown Residential Density Chart.

(D) Allowed Uses and Basic Development Standards.

The following table contains the basic zoning regulations that apply to uses. To use the chart, read down the left-hand column titled “Use.” When you have located the use that interests you, read across to find regulations that apply to that use. Uses are permitted unless otherwise specified in the Special Regulations column. Permitted uses may require land use permit approval. See RZC 21.76.020, Overview of the Development Process, for more information. Uses not listed are not permitted.

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
Residential			
1	Multifamily Structure, Mixed-Use Residential	Dwelling Unit (1.0, 2.25) Plus one guest space per four units for projects with six units or more. Curbside parking along the site may be counted towards up to 25 percent of the required off-street parking.	<ol style="list-style-type: none"> 1. Maximum density per lot dependent upon size and width of lot, per RZC 21.10.130(B), Downtown Residential Densities Chart. 2. Not permitted on ground floor street fronts of Type I pedestrian streets (see Map 10.3, Downtown Pedestrian System Map), or where ground floor residences may be negatively impacted by nearby nonresidential uses, except through establishment of an Administrative Design Flexibility per RZC 21.76.070(C). Residential uses may be allowed on ground floor streets fronts of Type II Pedestrian Streets per RZC 21.62.020(F)(5), Ground Floor Residential Uses on Type II Pedestrian Streets, but not within the shorter distance of 100 feet or one-quarter block-length from a street intersection. 3. Affordable Housing requirements apply to developments of 10 units or more. See RZC 21.20.020. 4. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
2	Dormitory	Bed (0.5, 1.0)	<u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
3	SRO		

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
4	Housing Services for the Elderly	See Special Regulations.	<p>Parking requirements are as follows:</p> <ol style="list-style-type: none"> 1. Multifamily housing for senior citizens: Unit (0.5, 2.0). 2. Nursing home or long-term care facility: 4 patient beds (1.0, 1.0). 3. Retirement residence with no skilled nursing facility: Unit (1.0, 1.0). 4. Retirement residence with skilled nursing facility: Worker on largest shift (1.25, 1.25). 5. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 6. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>

General Sales and Services			
5	General Sales or Services	1,000 sq ft gfa (2.0, 3.5)	<ol style="list-style-type: none"> 1. Uses not permitted include: <ol style="list-style-type: none"> a. Gasoline service. b. Auto repair. c. Automobile sales with outdoor display and storage. d. Rental storage and mini-warehouses. e. Retail sales or services involving drive-through/drive-up facilities, except drive-through facilities confined within the garage of a multi-story (at least three stories) building shall be allowed when the drive-through lanes provide a queuing length adequate to serve peak demand without overflowing onto public sidewalks or streets, as determined by a professional traffic engineer and is approved by the City. 2. Shall not be materially detrimental in terms of noise, truck traffic and other potential operational impacts with nearby multi-story mixed-use/residential developments. 3. Parking standards for restaurant uses: <ol style="list-style-type: none"> a. Sit-down restaurant: 1,000 SF GFA (9.0, 9.0). b. Take-out restaurant: 1,000 SF GFA (10.0, 10.0). c. 1,000 sq ft gfa (5.0, 5.0) when the restaurant is located in a multi-story building of at least three stories tall. d. The Technical Committee may waive the parking requirement for restaurant/deli/café uses less than 750 square feet gfa that support/enhance the City's vision for creating/enhancing Downtown as a pedestrian place provided: <ol style="list-style-type: none"> i. The use is located in an office building and primarily serves the occupants and guests of the office building; or ii. The use is visible from, and within 100 feet of a promenade or Downtown park, such as Luke McRedmond Park, Anderson Park, O'Leary Park, The Edge Skate Park or the 83rd Street Promenade, for example, or within 100 feet of a critical areas buffer of the Sammamish River and access to the River Trail, and the use is designed to enliven the pedestrian environment and primarily cater to pedestrians and outdoor patrons. 4. The maximum number of parking stalls allowed may be increased to 5.0 per 1,000 square feet of gross floor area for the retail components of mixed-use developments. 5. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
6	Hotels, Motels, and Other Accommodation Services	Rental room (1.0, 1.0)	
Transportation, Communication, Information, and Utilities			
7	Road, Ground Passenger and Transit Transportation	1,000 sq ft gfa	1. Office only. No vehicle storage.
8	Rapid charging station	Adequate to accommodate peak use	1. Shall not be located on a parcel that abuts a residential zone.
9	Battery exchange station		

10	Communications and Information		
11	Local Utilities		
12	Large Satellite Dishes/Amateur Radio Antenna(s)	Adequate to accommodate peak use	See RZC 21.56 Wireless Communications Telecommunications Facilities.
13	Wireless Communication Facilities		
14	Broadcast and Relay Towers		1. Requires a conditional use permit. See RZC 21.76.070(K) Conditional Use Permit.
Arts, Entertainment, and Recreation			
15	Arts, Entertainment, and Recreation	Adequate to accommodate peak use	
Education, Public Administration, Health Care, and other Institutions			
16	Education, Public Administration, Health Care, and other Institutions, except those listed below	See Special Regulations.	<ol style="list-style-type: none"> 1. Parking: The number of spaces must be adequate to accommodate the peak customer and employee shift, demonstrated by a parking study or other study submitted by the applicant and approved by the Code Administrator. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,
17	Day Care Center	See Special Regulations	<ol style="list-style-type: none"> 1. Provisions for day care centers include: <ol style="list-style-type: none"> a. Shall provide parking as follows: Employee on maximum shift (1.0, 1.0). b. Play equipment shall be located no less than 10 feet from any property line. c. Parking: The number of spaces must be adequate to accommodate the peak shift as determined by the Code Administrator after considering the probable number of employees, etc. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,
18	Religious Institutions	1,000 sq ft gfa (10.0, 10.0) or 1/5 fixed seats	<ol style="list-style-type: none"> 1. A seat is one fixed seat or 18 inches on a pew or bench or seven square feet in the general assembly area (including aisle space, but excluding stage, podium, lobby, and space for musical instruments). 2. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 3. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,
19	Funeral Homes and Services		<ol style="list-style-type: none"> 1. Excludes crematoriums. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,
Other			

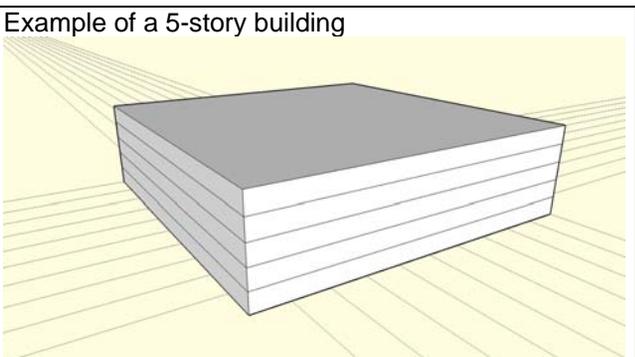
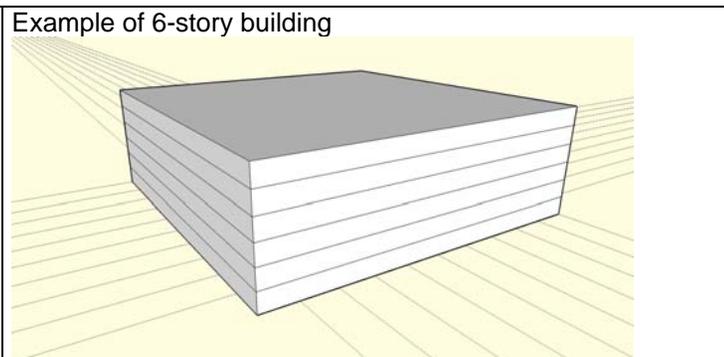
20	Vending Carts, Kiosks		<ol style="list-style-type: none"> 1. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. 2. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. 3. Structures shall be secured to prevent tipping and endangering public safety. 4. Maximum size is six feet wide by ten feet long. Administrative design review required for structures.
21	Automobile Parking Facilities		<ol style="list-style-type: none"> 1. Surface parking lots are prohibited.

21.10.090 River Bend (RVBD) Zone

(A) Purpose.

The River Bend (RVBD) zone is one of four distinct mixed-use residential/office districts in the Downtown Neighborhood: Sammamish Trail (SMT), Town Square (TSQ), Anderson Park (AP), and River Bend (RVBD). All four zones are intended to provide for significant residential growth, as well as opportunities for growth in professional, business, health, and personal services. These zones provide for a range of employment uses, such as financial and professional services, public administrative offices, health services, advanced technology industries, universities and technical colleges, and other activities normally conducted in multi-story office structures. These zones are intended as areas for the densest employment and residential uses in the Downtown, but also provide for supporting retail, service, and entertainment uses located within walking distance of each other. Some of the quieter streets in these zones allow residential uses on the ground floor, whereas the primary streets are intended for active, pedestrian-friendly and activating commercial uses. The River Bend zone enhances this area as an entrance to downtown by requiring streetscape improvements, by using design standards to encourage the creation of mixed residential/office villages and buildings, and by linking the zone with the Downtown core and Sammamish River. The River Bend zone also preserves the “green” gateway on Leary Way at the south end of Downtown by providing for the acquisition of land and the application of design standards and forest management.

(B) Maximum Development Yield.

Allowed	Base	Maximum	Example of a 5-story building	Example of 6-story building
Height	5 stories	6 stories		
Lot Coverage	100 percent	100 percent		

These are office building examples using Transfer Development Rights [or Green Building Program](#) to achieve the maximum achievable floor area within the maximum allowed building height. Residential and mixed-use residential developments may have similar height, but volume will differ due to setback and open space requirements

(C) Regulations Common to All Uses.

Regulation	Standard	Notes and Exceptions
Front Setback (distance from back of curb)		
Front and side street (commercial use)	See Map 10.3 Downtown Pedestrian System Map	1. Setbacks along Downtown Streets are regulated by the Downtown Pedestrian System which specifies street frontage standards between the street curb and the face of buildings, depending on site location.
Front and side street (residential use on ground floor)	n/a	Not permitted on ground floor street fronts of Type I pedestrian streets (Map 10.3, Downtown Pedestrian System Map).
Setback line (distance from property line)		
Side commercial	0 feet	
Rear commercial	0 feet	
Side (residential)	See RZC 21.10.130(D) Residential Setback Requirements	
Rear (residential)	10 feet	
Yard adjoining BNSF ROW or Parks	14 feet	
Yard adjoining Mid-Block Path	See Map 10.3 Downtown Pedestrian System Map	
Other Standards		
Minimum Building Height	n/a	
Maximum Building Height w/o TDRs or GBP	5 Stories	
Maximum Building Height w/ TDRs or GBP	6 Stories	1. One floor of additional height may be achieved with the use of Transfer Development rights.. (See RZC 21.10.160, Using Transfer Development Rights) or through compliance with the Green Building Program (See RZC 21.67, Green Building Infrastructure Program)
Maximum Height Within Shorelines (SMP)	35 feet	1. This height limit is restricted to that portion of the building physically located within the shoreline jurisdiction. (SMP) 2. The maximum height of structures, including bridges, that support a regional light rail transit system may be higher than 35 feet but shall be no higher than is reasonably necessary to address the engineering, operational, environmental and regulatory issues at the location of the structure. (SMP)
Maximum Lot Coverage	100 percent	1. For residential development without ground floor commercial/office, lot coverage shall be governed by RZC 21.10.130(B), Residential Density

		Chart.
Base FAR Without TDRs or GBP	1.25	<p>1. Maximum FAR without TDRs or GBP, for nonresidential space, is 1.25. Residential space within a mixed-use building is exempt from TDR or GBP requirements. See RZC 21.10.160, Using Transfer of Development Rights and RZC 21.67, Green Building Infrastructure Program.</p> <p>2. All legal lots are entitled to 10,000 square feet GFA without the use of TDRs provided that other site requirements can be met.</p>
Allowed Residential Density	Depends on Lot Size	See RZC 21.10.130(B), Downtown Residential Density Chart.

(D) Allowed Uses and Basic Development Standards.

The following table contains the basic zoning regulations that apply to uses. To use the chart, read down the left-hand column titled “Use.” When you have located the use that interests you, read across to find regulations that apply to that use. Uses are permitted unless otherwise specified in the Special Regulations column. Permitted uses may require land use permit approval. See RZC 21.76.020, Overview of the Development Process, for more information. Uses not listed are not permitted.

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
Residential			
1	Multifamily Structure, Mixed-Use Residential	Dwelling Unit (1.0, 2.25) Plus one guest space per four units for projects with six units or more. Curbside parking along the site may be counted towards up to 25 percent of the required off-street parking.	<ol style="list-style-type: none"> 1. Maximum density per lot dependent upon size and width of lot, per RZC 21.10.130(B), Downtown Residential Densities Chart. 2. Not permitted on ground floor street fronts of Type I pedestrian streets (see Map 10.3, Downtown Pedestrian System Map), or where ground floor residences may be negatively impacted by nearby nonresidential uses, except by establishment of an Administrative Design Flexibility per RZC 21.76.070(C). Residential uses may be allowed on ground floor streets fronts of Type II Pedestrian Streets per RZC 21.62.020(F)(5), Ground Floor Residential Uses on Type II Pedestrian Streets, but not within the shorter distance of 100 feet or one-quarter block-length from a street intersection. 3. Affordable Housing requirements apply to developments of 10 units or more. See RZC 21.20.020. 4. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
2	Dormitory	Bed (0.5, 1.0)	The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
3	SRO		

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
4	Housing Services for the Elderly	See Special Regulations.	<p>Parking requirements are as follows:</p> <ol style="list-style-type: none"> 1. Multi-family housing for senior citizens: Unit (0.5, 2.0). 2. Nursing home or long-term care facility: 4 patient beds (1.0, 1.0). 3. Retirement residence with no skilled nursing facility: Unit (1.0, 1.0). 4. Retirement residence with skilled nursing facility: Worker on largest shift (1.25, 1.25). 5. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 6. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>

General Sales or Service			
5	General Sales or Services	1,000 sq ft gfa (2.0, 3.5)	<ol style="list-style-type: none"> 1. Uses not permitted include: <ol style="list-style-type: none"> a. Rental storage and mini-warehouses. b. Retail sales or services involving drive-through/drive-up facilities, except drive-through facilities confined within the garage of a multi-story (at least three stories) building shall be allowed when the drive-through lanes provide a queuing length adequate to serve peak demand without overflowing onto public sidewalks or streets, as determined by a professional traffic engineer and approved by the City. 2. Shall not be materially detrimental in terms of noise, truck traffic and other potential operational impacts with nearby multi-story mixed-use/residential developments. 3. Auto Repair without outdoor storage and outdoor service is allowed provided: <ol style="list-style-type: none"> a. All service/repair work is performed indoors. b. There is no overnight storage of customer vehicles in outdoor parking areas. 4. Parking standards for restaurant uses: <ol style="list-style-type: none"> a. Sit-down restaurant: 1,000 SF GFA (9.0, 9.0). b. Take-out restaurant: 1,000 SF GFA (10.0, 10.0). c. 1,000 sq ft gfa (5.0, 5.0) when the restaurant is located in a multi-story building at least three stories tall. d. The Technical Committee may waive the parking requirement for restaurant/deli/café uses less than 750 square feet gfa that support/enhance the City's vision for creating /enhancing Downtown as a pedestrian place provided: <ol style="list-style-type: none"> i. The use is located in an office building and primarily serves the occupants and guests of the office building; or ii. The use is visible from and within 100 feet of a promenade or Downtown park, such as Luke McRedmond Park, Anderson Park, O'Leary Park, The Edge Skate Park or the 83rd Street Promenade, for example, or within 100 feet of a critical areas buffer of the Sammamish River and access to the River Trail, and the use is designed to enliven the pedestrian environment and primarily cater to pedestrians and outdoor patrons. 5. The maximum number of parking stalls allowed may be increased to 5.0 per 1,000 square feet of gross floor area for the retail components of mixed-use developments. <u>6. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
6	Hotels, Motels, and Other Accommodation Services	Rental room (1.0, 1.0)	<u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
Transportation, Communication, Information, and Utilities			
7	Road, Ground Passenger & Transit Transportation	1,000 sq ft gfa (2.0, 3.5)	1. Regional light rail transit system and office uses only. No vehicle storage except light rail vehicles.

8	Rapid charging station	Adequate to accommodate peak use	1. Shall not be located on a parcel that abuts a residential zone.
9	Battery exchange station		
10	Communications and Information	1,000 sq ft gfa (2.0, 3.5)	
11	Local Utilities		
12	Large Satellite Dishes/Amateur Radio Antenna(s)	Adequate to accommodate peak use	See RZC 21.56 Wireless Communications Facilities.
13	Wireless Communication Facilities		1. Requires a conditional use permit. See RZC 21.76.070(K) Conditional Use Permit.
14	Broadcast and Relay Towers		
Arts, Entertainment, and Recreation			
15	Arts, Entertainment, and Recreation	Adequate to accommodate peak use	
Education, Public Administration, Health Care, and other Institutions			
16	Education, Public Administration, Health Care, and other Institutions, except those listed below	See Special Regulations.	<ol style="list-style-type: none"> 1. Parking: The number of spaces must be adequate to accommodate the peak customer and employee shift, demonstrated by a parking study or other study submitted by the applicant and approved by the Code Administrator. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
17	Day Care Center		<ol style="list-style-type: none"> 1. Provisions for day care centers include: <ol style="list-style-type: none"> a. Shall provide parking as follows: Employee on maximum shift (1.0, 1.0). b. Play equipment shall be located no less than 10 feet from any property line. c. Parking: The number of spaces must be adequate to accommodate the peak shift as determined by the Code Administrator after considering the probable number of employees, etc. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
18	Religious Institutions	1,000 sq ft gfa (10.0, 10.0) or 1/5 fixed seats	<ol style="list-style-type: none"> 1. A seat is one fixed seat or 18 inches on a pew or bench or seven square feet in the general assembly area (including aisle space, but excluding stage, podium, lobby, and space for musical instruments). 2. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 3. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.

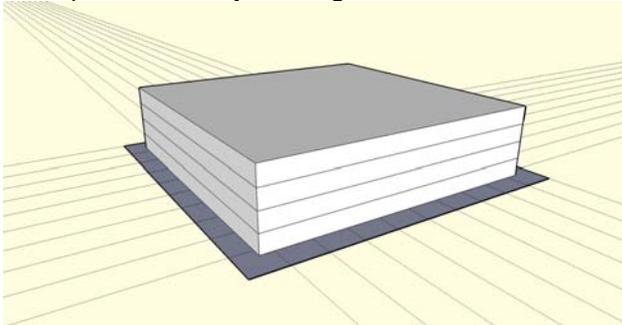
19	Funeral Homes and Services	1,000 sq ft gfa (10.0, 10.0) or 1/5 fixed seats	<ol style="list-style-type: none"> 1. Excludes crematoriums. 2. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,
Other			
20	Vending Carts, Kiosks		<ol style="list-style-type: none"> 1. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. 2. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. 3. Structures shall be secured to prevent tipping and endangering public safety. 4. Maximum size is six feet wide by ten feet long. 5. Administrative design review required for structures.
21	Automobile Parking Facilities		<ol style="list-style-type: none"> 1. Surface parking lots are prohibited.

21.10.100 River Trail (RVT), Carter (CTR) , and East Hill (EH) Zones

(A) Purpose.

Downtown includes three residential zones at the periphery of the neighborhood that are intended to retain a quieter “residential” character than the other nearby mixed-use areas. These zones will provide a variety of housing types that are not primarily mixed-use in developments that include more typical residential features, such as front yards, landscaping, and ground-related patios and porches. These areas are all located within walking distance to the various retail and service areas in the Downtown. The regulations in this division are intended to retain the East Hill zone’s special character and to ensure that single-family residential structures in this zone are well maintained until they are redeveloped with higher-density residential uses or are converted to nonresidential uses that are compatible with the residential neighborhood.

(B) Maximum Development Yield.

Allowed	Base	Maximum	<p>Example of a 4-story building</p> 
Height	For stories	5 stories	
Lot Coverage	75 percent	Depends on setbacks and residential usable space requirements	
<p>These are office building examples using Transfer Development Rights or GBP to achieve the maximum achievable floor area within the maximum allowed building height. Residential and mixed-use residential developments may achieve similar results.</p>			

(C) Regulations Common to All Uses.

Regulation	Standard	Notes and Exceptions
Front Setback (distance from back of curb)		
Front and side street (commercial use)	See Map 10.3 Downtown Pedestrian System Map.	Setbacks along Downtown Streets are regulated by the Downtown Pedestrian System which specifies street frontage standards between the street curb and the face of buildings, depending on site location.
Front and side street (residential use on ground floor)	See Map 10.3 Downtown Pedestrian System Map.	Setbacks along Downtown Streets are regulated by RZC 21.10.150, Downtown Pedestrian System which specifies street frontage standards between the street curb and the face of buildings, depending on site location.
Setback line (distance from property line)		
Side commercial	0 feet	
Rear commercial	0 feet	
Side (residential)	Depends on size of building	See RZC 21.10.130(D) Residential Setback Requirements
Rear (residential)	10 feet	
Yard adjoining BNSF ROW or Parks	14 feet	
Yard adjoining Mid-Block Path	See Pedestrian System Map	
Other Standards		
Minimum Building Height	n/a	
Maximum Building Height without TDRs <u>or GBP</u>	4 Stories	
Maximum Building Height with TDRs <u>or GBP</u>	5 Stories	1. One floor of additional height may be achieved with the use of Transfer Development rights. (See RZC 21.10.160, Using Transfer Development Rights) or through compliance with the Green Building Program (See RZC 21.67, Green Building Infrastructure Program)
Maximum Height Within Shorelines (SMP)	35 feet	1. This height limit is restricted to that portion of the building physically located within the shoreline jurisdiction. (SMP) 2. The maximum height of structures, including bridges, that support a regional light rail transit system may be higher than 35 feet but shall be no higher than is reasonably necessary to address the engineering, operational, environmental and regulatory issues at the location of the structure. (SMP)
Maximum Lot Coverage	See Residential Density Chart.	1. For residential development without ground floor commercial/office, lot coverage shall be governed by RZC 21.10.130(B), Residential Density Chart. 2. For non-residential uses, maximum allowable lot coverage is 75%.
Base FAR Without TDRs <u>or GBP</u>	1.25	1. Maximum FAR without TDRs <u>or GBP</u> , for non-residential space, is 1.25. Residential space within a mixed-use building is exempt from TDR <u>or GBP</u> requirements. See RZC 21.10.160, Using Transfer Development Rights <u>and</u> RZC 21.67, Green Building Infrastructure Program 2. All legal lots are entitled to 10,000 square feet GFA without the use of

		TDRs or GBP provided that other site requirements can be met.
Allowed Residential Density	Depends on Lot Size	See RZC 21.10.130(B), Downtown Residential Density Chart.

(D) Allowed Uses and Basic Development Standards.

The following table contains the basic zoning regulations that apply to uses. To use the chart, read down the left-hand column titled "Use." When you have located the use that interests you, read across to find regulations that apply to that use. Uses are permitted unless otherwise specified in the Special Regulations column. Permitted uses may require land use permit approval. See RZC 21.76.020, Overview of the Development Process, for more information. Uses not listed are not permitted.

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
Residential			
1	Multifamily Structure, Mixed-Use Residential	Dwelling Unit (1.0, 2.25) Plus one guest space per four units for projects with six units or more. Curbside parking along the site may be counted towards up to 25 percent of the required off-street parking.	<ol style="list-style-type: none"> 1. Maximum density per lot dependent upon size and width of lot, RZC 21.10.130(B), Downtown Residential Densities Chart. 2. Not permitted on ground floor street fronts of Type I pedestrian streets (see Map 10.3, Downtown Pedestrian System Map), or where ground floor residences may be negatively impacted by nearby nonresidential uses, except through establishment of an Administrative Design Flexibility per RZC 21.76.070(C). Residential uses may be allowed on ground floor streets fronts of Type II Pedestrian Streets per RZC 21.62.020(F)(5), Ground Floor Residential Uses on Type II Pedestrian Streets, but not within the shorter distance of 100 feet or one-quarter block length from a street intersection. 3. Affordable Housing requirements apply to developments of 10 units or more. See RZC 21.20.020. 4. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
2	Dormitory		The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
3	SRO	Bed (0.5, 1.0)	The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.

Section	Use	Parking ratio: unit of measure (min. required, max. allowed)	Special Regulations
4	Housing Services for the Elderly	See Special Regulations.	<p>Parking requirements are as follows:</p> <ol style="list-style-type: none"> 1. Multifamily housing for senior citizens: Unit (0.5, 2.0). 2. Nursing home or long-term care facility: 4 patient beds (1.0, 1.0). 3. Retirement residence with no skilled nursing facility: Unit (1.0, 1.0). 4. Retirement residence with skilled nursing facility: Worker on largest shift (1.25, 1.25). 5. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. <u>6. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>

General Sales or Service			
5	General Sales or Services	1,000 sq ft gfa (2.0, 3.5)	<ol style="list-style-type: none"> 1. Uses not permitted include: <ol style="list-style-type: none"> a. Gasoline service. b. Auto repair. c. Automobile sales with outdoor display and storage. d. Rental storage and mini-warehouses. e. Retail sales or services involving drive-through/drive-up facilities. g. Uses requiring or utilizing outdoor storage. 2. Shall not be materially detrimental in terms of noise, truck traffic and other potential operational impacts with nearby residential developments. 3. General Retail uses may only occupy single-family structures in existence prior to the year 2005 provided: a) Required parking for the use is not located in the front yard, and the parking and driveway areas do not expand beyond the year 2005 recorded parcel; b) The exterior of the structure, accessory structures, and landscaping maintain a single-family character; and c) Storage of all products is kept indoors and accessory buildings do not exceed 60 percent of the ground floor area of the main structure 4. General Services are allowed only on the ground floor of multi-story mixed-use residential buildings. Except, General Service uses may occupy existing single-family structures in existence prior to 2005 provided: a) on-site parking is not located in the front yard; b) the exterior of the structure and landscaping maintains a single-family character; and c) the use is consistent with Comprehensive Plan Policies for the zone. 5. Parking standards for restaurant uses: <ol style="list-style-type: none"> a. Sit-down restaurant: 1,000 SF GFA (9.0, 9.0). b. Take-out restaurant: 1,000 SF GFA (10.0, 10.0). c. The Technical Committee may waive the parking requirement for restaurant/deli/café uses less than 750 square feet gfa that support/enhance the City's vision for creating /enhancing Downtown as a pedestrian place provided: <ol style="list-style-type: none"> i. The use is located in an office building and primarily serves the occupants and guests of the office building; or ii. The use is visible from and within 100 feet of a promenade or Downtown park, such as Luke McRedmond Park, Anderson Park, O'Leary Park, The Edge Skate Park or the 83rd Street Promenade, for example, or within 100 feet of a critical areas buffer of the Sammamish River and access to the River Trail, and the use is designed to enliven the pedestrian environment and primarily cater to pedestrians and outdoor patrons. 6. The maximum number of parking stalls allowed may be increased to 5.0 per 1,000 square feet of gross floor area for the retail components of mixed-use Developments. 7. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013,</u>
Transportation, Communication, Information, and Utilities			

6	Road, Ground Passenger and Transit Transportation		<ol style="list-style-type: none"> 1. Office only, No vehicle or outside storage allowed. 2. These uses may occupy existing single-family structures in existence prior to 2005 provided: 1) on-site parking is not located in the front yard; 2) the exterior of the structure and landscaping maintains a single-family character; and 3) the use is consistent with Comprehensive Plan Policies for the zone.
7	Communications and Information	1,000 sq ft gfa (2.0, 3.5)	<ol style="list-style-type: none"> 1. Office only. No vehicle or outside storage allowed. 2. These uses may occupy existing single-family structures in existence prior to 2005 provided: a) on-site parking is not located in the front yard; b) the exterior of the structure and landscaping maintains a single-family character; and c) the use is consistent with Comprehensive Plan Policies for the zone.
8	Local Utilities		<ol style="list-style-type: none"> 1. Office only, No vehicle or outside storage allowed. 2. These uses may occupy existing single-family structures in existence prior to 2005 provided: a) on-site parking is not located in the front yard; b) the exterior of the structure and landscaping maintains a single-family character; and c) the use is consistent with Comprehensive Plan Policies for the zone.
9	Large Satellite Dishes/Amateur Radio Antenna(s)	Adequate to accommodate peak use	See RZC 21.56 Wireless Communications Facilities.
10	Wireless Communication Facilities		
11	Broadcast and Relay Towers		1. Requires a conditional use permit. See RZC 21.76.070(K) Conditional Use Permit.
Arts, Entertainment, and Recreation			
12	Arts, Entertainment, and Recreation	Adequate to accommodate peak use	<ol style="list-style-type: none"> 1. Limited to parks and community centers, except events and exhibits are allowed on the ground floor of multi-story mixed use residential buildings. 2. Art galleries are allowed in the ground floor of multi-story mixed-use residential buildings and in single-family structures in existence prior to the year 2005 provided: a) Required parking for the use is not located in the front yard and the parking and driveway areas do not expand beyond the year 2005 recorded parcel; b) The exterior of the structure, accessory structures, and landscaping maintain a single-family character. 3. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.
Education, Public Administration, Health Care, and other Institutions			
13	Education, Public Administration, Health Care, and other Institutions, except those listed below	See Special Regulations.	<ol style="list-style-type: none"> 1. Parking: The number of spaces must be adequate to accommodate the peak customer and employee shift, demonstrated by a parking study or other study submitted by the applicant and approved by the Administrator. 2. These uses may occupy existing single-family structures in existence prior to 2005 provided: a) on-site parking is not located in the front yard; b) the exterior of the structure and landscaping maintains a single-family character; and c) the use is consistent with Comprehensive Plan Policies for the zone. 3. The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.

14	Day Care Center		<ol style="list-style-type: none"> 1. Provisions for day care centers include: <ol style="list-style-type: none"> a. Shall provide parking as follows: Employee on maximum shift (1.0, 1.0). b. Play equipment shall be located no less than 10 feet from any property line. c. Parking: The number of spaces must be adequate to accommodate the peak shift as determined by the Administrator after considering the probable number of employees, etc. 2. <u>These uses may occupy existing single-family structures in existence prior to 2005 provided: a) on-site parking is not located in the front yard; b) the exterior of the structure and landscaping maintains a single-family character; and c) the use is consistent with Comprehensive Plan Policies for the zone.</u> 3. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
15	Religious Institutions	1,000 sq ft gfa (10.0, 10.0) or 1/5 fixed seats	<ol style="list-style-type: none"> 1. These uses are may occupy existing single-family structures in existence prior to 2005 provided: a) on-site parking is not located in the front yard; b) the exterior of the structure and landscaping maintains a single-family character; and c) the use is consistent with Comprehensive Plan Policies for the zone. 2. A seat is one fixed seat or 18 inches on a pew or bench or seven square feet in the general assembly area (including aisle space, but excluding stage, podium, lobby, and space for musical instruments). 3. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. 4. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>
16	Funeral Homes and Services		<ol style="list-style-type: none"> 1. Excludes crematoriums. 2. <u>These uses are may occupy existing single-family structures in existence prior to 2005 provided: a) on-site parking is not located in the front yard; b) the exterior of the structure and landscaping maintains a single-family character; and c) the use is consistent with Comprehensive Plan Policies for the zone.</u> 3. <u>The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.</u>

21.10.110 Building Height

- (A) Height Tradeoff. The maximum building height on a site may be exceeded when building height reductions are required at building edges, along a street or park, to achieve better design and stepped building height through the land use permit process. The amount of floor area that is allowed to exceed the prescribed maximum building height (without use of bonuses or transfer of development rights) shall not exceed the floor area that was removed or omitted to create the stepped building facade and shall not exceed one additional floor above the prescribed maximum building height.
- (B) Height Limit Overlay. Building height restrictions within the Height Limit Overlay (HL) areas shall be as indicated on the Height Limit Overlays Map. The designation HL followed by a number indicates the maximum number of floors allowed. The limits within the overlay areas may not be exceeded with the use of bonuses or transfer development rights.

INSERT MAP 10.2, HEIGHT LIMIT OVERLAY MAP, HERE

21.10.120 Parking Standards

- (A) Developments may provide parking in excess of the maximum allowed parking standard shown in the Allowed Uses and Basic Development Standards Tables in RZC 21.10.030 through RZC 21.10.100, above, provided the excess parking is also available at all times to the general public, and there is signage at the facility to inform users which parking stalls are available for public use.
- (B) For residential uses, curbside parking along the site may be counted towards up to 25 percent of the required off-street parking.
- (C) A floor area bonus is available in the Old Town zone when utilizing the fee in-lieu of parking provision.
- (D) Parking is not required for storefront ground floor home office space if the space is the lesser of 450 square feet or 25 percent of the gross floor area of the residence the home office is part of.
- (E) [The number of required parking stalls may be reduced by 25% without the requirement for a parking study if the development is located within a ¼ mile of a transit through the GBP which ends on January 1, 2013.](#)

21.10.130 Downtown Residential Site Requirements

- (A) Purpose. The purpose of the Residential Site Requirements is to:
 - (1) ensure that Downtown residential development is in character with the goals and visions for the neighborhood in terms of density and bulk on the sites and the blocks they are located in, as well as livability and comfort within developments and compatibility between neighboring properties; and
 - (2) create light, air, pedestrian and vehicular circulation, emergency access, and general aesthetics by providing setbacks for development; and
 - (3) provide open space in the Downtown area that is usable by its residents and visitors.
- (B) Downtown Residential Density Chart
 - (1) Use the table below to determine the permitted residential density of a site. The table is read from left to right. For example, in the East Hill District, a site that is 12,000 square feet in area and 120 feet in width will be allowed up to 45 dwellings per acre. If the site's area and width are in different rows, for example, 12,000 square feet and 100 feet wide, the lower density shall apply (36 dwelling units per acre).
 - (2) Exceptions.
 - (a) If the site's width would allow a higher density than the site's area, the higher density shall be allowed if the site's area is at least 85 percent of the lowest range of the areas in the same row as the site's width. Example: On a site in the East Hill District where the lot width equals 120 feet and the site area is at least 10,200 square feet (85 percent of 12,000 square feet), 45 dwelling units per acre are allowed.
 - (b) If the site's area would allow a higher density than the site's width, the higher density shall be allowed if the site's width is at least 85 percent of the lowest range of the widths in the same row as the site's area. Example: On a site in the East Hill District where the lot area equals 12,000 square feet and the site width is at least 102 feet (85 percent of 120 feet), 45 dwelling units per acre are allowed.
 - (3) Lot coverage requirements do not apply to residential developments with ground floor commercial/retail uses.
 - (4) Bonus for Semi- and Full-Subterranean Parking. In freestanding residential developments that are on sites which are not large enough to qualify for densities that are not regulated by a maximum number of dwelling units per acre, but by the bulk, height, parking, and other standards as shown in the Downtown Residential Densities Chart, below, a density bonus of 10 percent shall be granted to developments that provide semi- or full-subterranean parking where the finished floor height of the first floor is not more than four feet above the street curb.
 - (5) For Unit Lot Subdivisions in the Sammamish Trail and Town Square zones the minimum residential density required shall be 35 dwelling units per acre. See RZC 21.74 Land Division.

Downtown Residential Densities Chart				
Districts	Minimum Site Area (Sq. Ft.)	Minimum Site Width (Ft.)	Maximum Dwelling Units per Net Acres	Maximum Allowable Lot Coverage
River Trail, Carter and East Hill	2,400 – 3,199	Less than 26	1 unit per lot	75 percent
	2,400 – 3,199	27 – 43	2 units per lot With alley access only. Otherwise one unit per lot.	75 percent
	3,200 – 5,999	44 – 49	30 du/ac	75 percent
	6,000 – 11,999	50 – 119	36 du/ac	N/A
	12,000 – 17,999	120 – 179	45 du/ac	N/A
	18,000 or more	180 or more	Density shall not be regulated by a maximum number of dwelling units per acre, but by the application of bulk, height, parking, and all other development standards. For example, developments may achieve as many dwellings as possible when the development provides all of the necessary parking, open space, etc., for the number of units proposed, and the building(s) meet all of the prescribed standards. All developments with proposed densities exceeding 66 dwelling units per acre, inclusive of density bonuses allowed per Section, Residential Density Bonus, shall be required to have at least one level of semi-subterranean or subterranean parking or structured parking hidden behind ground floor commercial space or other pedestrian amenities along the street front.	N/A
Sammamish Trail, Town Square, Old Town, Anderson Park, River Bend and Town Center	2,400 – 3,199	Less than 26	1 unit per lot	75 percent
	2,400 – 3,199	27 – 43	2 units per lot With alley access only. Otherwise one unit per lot.	75 percent
	3,200 – 5,999	44 – 49	30 du/ac	75 percent
	6,000 – 11,999	50 – 119	36 du/ac	N/A
	12,000 or more	120 or more	Density shall not be regulated by a maximum number of dwelling units per acre, but by the application of bulk, height, parking, and all other development standards. For example, developments may achieve as many dwellings as possible when the development provides all of the necessary parking, open space, etc., for the number of units proposed, and the building(s) meet all of the prescribed standards. All developments with proposed densities exceeding 66 dwelling units per acre, inclusive of density bonuses allowed per Section, Residential Density Bonus, shall be required to have at least one level of semi-subterranean or subterranean parking or structured parking hidden behind ground floor commercial space or other pedestrian amenities along the street front.	N/A

Trestle, Valley View and Bear Creek	2,400 – 3,199	Less than 30	1 unit per lot
	2,400 – 3,199	30 – 43	2 units per lot With alley access only. Otherwise one unit per lot.
	3,200 – 5,999	44 – 49	26 du/ac
	6,000 or more	50 or more	<p>Developments qualifying for additional height per RZC 21.10.060 (C) when infill/redevelopment of the site creates an urban village per RZC 21.62.020(G)(2)(c) the following shall apply.</p> <p>Density shall not be regulated by a maximum number of dwelling units per acre, but by the application of bulk, height, parking, and all other development standards. For example, developments may achieve as many dwellings as possible when the development provides all of the necessary parking, open space, etc., for the number of units proposed, and the building(s) meet all of the prescribed standards. All developments with proposed densities exceeding 66 dwelling units per acre, inclusive of density bonuses allowed per Section, Residential Density Bonus, shall be required to have at least one level of semi-subterranean or subterranean parking or structured parking hidden behind ground floor commercial space or other pedestrian amenities along the street front.</p> <p>Otherwise the maximum allowed density is 30 dwelling units per acre.</p>

(C) Residential Lot Coverage

- (1) Requirements. For residential developments without ground floor commercial/office uses, maximum lot coverage shall not exceed the standards in RZC 21.10.130(B), Downtown Residential Densities Chart.
- (2) Exemptions. The following structures or portions of structures shall be excluded from the measurement of lot coverage:
 - (a) Ramps or other access for the disabled or elderly meeting Washington State Rules and Regulations for Barrier-Free Design.
 - (b) Required pedestrian walkways/vehicular lanes per Downtown pedestrian system. The area devoted to the required pedestrian system between the building and curb line/property line shall be counted as open space.
 - (c) An underground structure (below natural grade), or underground portion of a structure, on any part of the entire lot that is landscaped on the surface.

(D) Residential Setback Requirements

- (1) Applicability. All lots shall have one front yard and one rear yard, with the exception of lots with frontage on two streets or access corridors, which will have two front yards and no rear yard. Where a residential use is located above a ground floor commercial/office use, the side and rear setbacks shall apply only to the residential use.
- (2) Permitted Structures in Required Setback Areas. The following features are permitted within front, rear and side yard setback areas, provided that they project no closer than five feet to a property line and do not project more than five feet into the setback area. Permitted projections include building extremities, such as balconies, patios, porches, architectural protrusions, subterranean garages and structures (below natural grade), bay windows, and chimneys. Except on Type I and II pedestrian streets per Section 30-040, projections may project over street sidewalks up to the property line provided they do not project more than five feet into the setback area. The projections may be further restricted through the land use permit process based on public safety and aesthetic considerations.
- (3) Front Setbacks. Front setbacks shall conform to the standards specified in the Pedestrian System Map.
- (4) Side Setbacks. Side setbacks shall be determined by structure depth and height, according to the following table. The depth of the building shall be measured at right angles, or as near to right angles as possible, from the front property line in a plane horizontal to the ground. Buildings built to the side property line, such as townhouses, and mixed-use residential buildings as shown in the photograph below, are exempt from this requirement at the property line.

Residential Side/Interior Setbacks			
Height of Side/Interior Façade at highest Point in Feet			
Structure Depth in Feet	<31'	31 – 40'	>40'
	Minimum Side Setback in Feet		
65' or less	5'	6'	7'
66' to 80'	6'	6'	8'
81' to 100'	8'	9'	11'
101' to 120'	11'	12'	14'
121' to 140'	14'	15'	17'
141' to 160'	17'	18'	20'
161' or more	19'	21'	23'

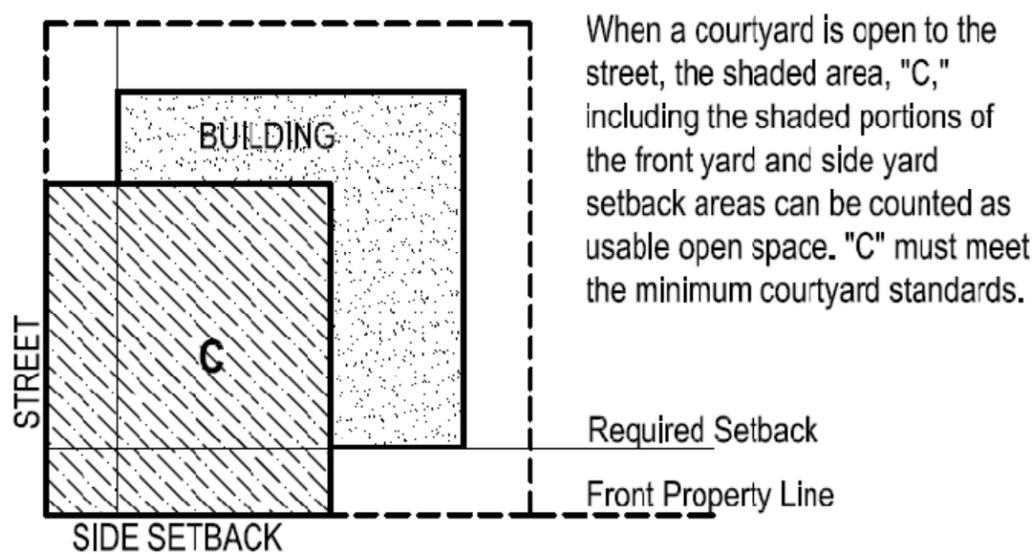


- (5) Rear Setbacks. Rear setbacks shall be provided for all residential uses per the Allowed Uses and Basic Development Standards Tables in RZC 21.10.030 through RZC 21.10.100, above.
- (6) Distance between Buildings.
 - (a) Two or more detached buildings on the same lot shall have a minimum separation of 15 feet. A detached accessory structure shall be separated by a minimum of five feet from any other structure. This restriction shall not apply to air conditioners, heaters, vents, pumps, solar collectors or similar equipment.
 - (b) Roof Eaves. Roof eaves of principal and accessory structures shall have a minimum separation of five feet. This restriction shall not apply to patio covers and similar structures with open lattice, grill work or uncovered roof beams.

(E) Residential Usable Open Space

- (1) General. On-site usable open space is an important feature for residential uses as it provides residents with a place, or places, to relax and/or recreate without the need to leave their building. In order to achieve some basic amounts of on-site usable open space, two types of usable open space are required for each unit: common open space in forms such as plazas, rooftop gardens, and recreation rooms; and private open spaces in the forms of balconies and patios. Below are the general standards for the two different types of usable open space:
 - (a) Common Usable Open Space. At least 100 square feet per residential unit of common usable open space shall be provided in the development, up to a maximum area equivalent to 20 percent of the site. The spaces provided shall meet the size and dimensional requirements specified in Table 21.10.130 (E), Residential Usable Open Space Sizes and Dimensions, below. Common usable open space may be provided in forms such as plazas, rooftop gardens, and recreation rooms that are accessible to all residents of a building. Units with at least 200 square feet of private usable open space (like townhomes), where the smallest dimension is no less than 10 feet, may be excluded from the count of units that need to contribute to the common usable open space requirement. Front yards may not be counted as common open space, except per Figure 10.1 below.

Figure 10.1



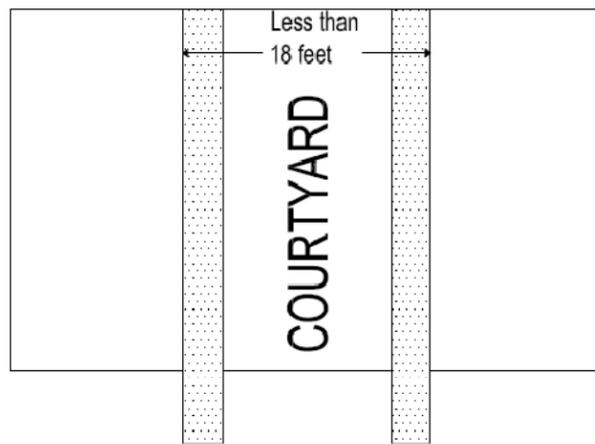
- (b) Private Usable Open Space. Private usable open space shall be provided in the form of an attached patio or balcony for each unit per Table 21.10.130 (E), Residential Usable Open Space Sizes and Dimensions below.
- (2) Open Space Size and Dimensions. Table 21.10.130 (E), Residential Usable Open Space Sizes and Dimensions specifies the minimum open space size and dimensions for both common and private usable open space areas. Balconies may be reduced to 12 square feet in area for up to 50 percent of the units when double doors are provided to the balcony.

Table 21.10.130 (E) Residential Usable Open Space Size and Dimensions				
Type of Usable Open Space	Minimum Length	Minimum Width	Minimum Height	Minimum Area (in sq. ft.)
1. Common	12'	12'	As specified in IBC for habitable overhead height	200
2. Private – Patio	8'	8'	Same as above	80
– Balcony	5'	5'	Same as above	50

(3) Use of In-Lieu Fee for Downtown Residential Usable Open Space

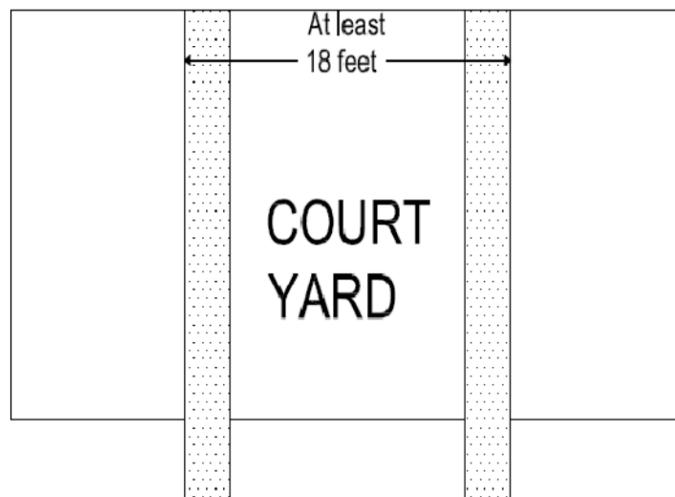
- (a) Balconies. If the street front facade of a building is deemed to be too cluttered, monotonous, and/or over dominated by too many balconies being too close together, the number of balconies on the facade may be reduced with the approval of the Design Review Board in order to effect a more balanced and attractive facade. An in-lieu fee for each required balcony not provided shall be paid to the City for park land purchase and improvements within the Downtown neighborhood. The fee for each balcony not provided on the building shall be equivalent to 50 percent of the park impact fee for a multi-family residence. No less than 50 percent of the units shall include private open spaces.
 - (b) Common Open Space. An in-lieu fee for each 100 square feet of common open space not provided shall be paid to the City for park land purchase and improvements within the Downtown neighborhood. The fee for each 100 square feet of required open space not provided on-site shall be equivalent to 50 percent of the park impact fee for a multi-family residence. No less than 50 square feet of common open space per unit shall be provided on-site.
- (4) Combining Common Usable Open Space and Pedestrian Access. Parking areas, driveways and pedestrian access shall not be counted as common usable open space. Except: if the total width of the common usable open space is 18 feet or wider, any pedestrian path or walkway traversing through the open space may be considered as common usable open space. See Figure 10.2 and Figure 10.3 below.

Figure 10.2



Area of walkway not counted as open space.

Figure 10.3



Area of walkway counted as open space.

21.10.140 Landscaping

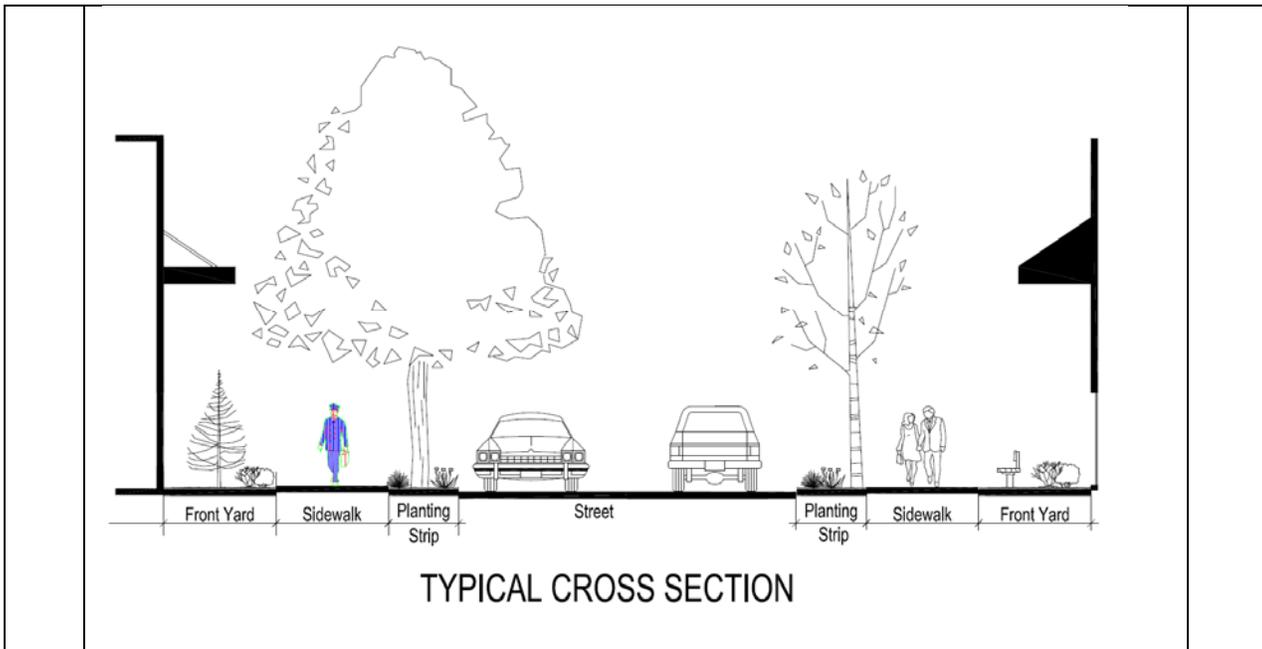
- (A) General Requirement. All setbacks, buffers, open spaces, pervious surfaces, plazas, parks, site and building entrances, pedestrian walkways, service areas, and parking lots shall be landscaped with plant materials. Existing vegetation may be maintained and apply toward this standard if the existing vegetation meets the landscaping requirements of this section, is healthy, and is likely to survive development. The requirements specified in RZC 21.60.040(C), Landscaping; RZC 21.62.020(F)(8), Residential Landscaping; and RZC 21.32 Landscaping, shall apply.
- (B) Plazas and common usable open spaces shall be landscaped to create visual interest by providing a variety of colors, heights, and forms of foliage, soften building edges, and reduce the impact of elements such as noise or wind.
- (C) The quantity of trees, shrubs, and other plant materials shall be designed to meet the size and function of the plaza or open space.

21.10.150 Pedestrian System

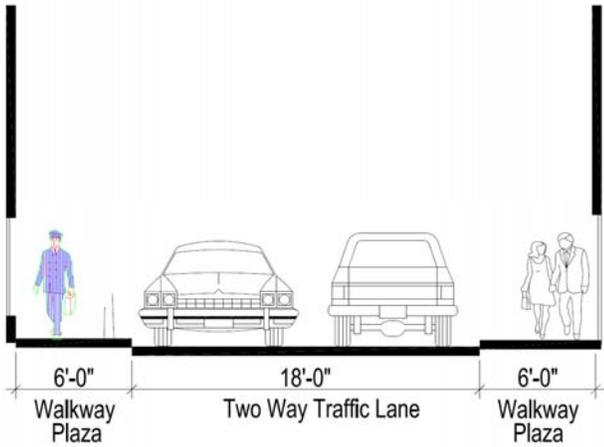
- (A) Purpose. The purpose of the Downtown pedestrian system is to:
 - (1) Provide safe pedestrian routes removed from traffic;
 - (2) Enhance the appearance of buildings and their settings;
 - (3) Provide a unified design element to complement varying architectural styles;
 - (4) Soften the appearance of parking lots and service storage areas; and
 - (5) Provide for the planting of street trees and other vegetation appropriate for an urban setting.
- (B) Installation of Pedestrian System. The various components of the pedestrian system are shown on Map 10.3 entitled "Downtown Pedestrian System"; the tables and graphics included in RZC 21.10.150(C), Pedestrian System Description; Map 10.4 entitled "Town Center Pedestrian System"; and the table RZC 21.10.150(O), entitled, "Downtown Streets Cross Sections"; all of which are incorporated as a part of this section. As property is developed or redeveloped, corresponding portions of the systems shall be installed or otherwise provided for by the property owner/developer. The front yard distance specifies the minimum front yard setback measured from the back of sidewalk. Where the front yard distance is specified as zero feet, the building shall be built to the back of the sidewalk. The mid-block segments shown on the map represent desired connections between blocks. In order to provide flexibility, the actual alignment shall be determined through the site plan land use permit process.

Insert map 10.3 pedestrian System

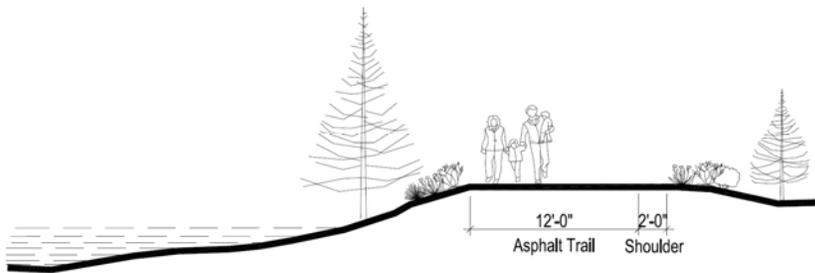
(C) Pedestrian System Description. The table and graphics below depict the various pedestrian system cross sections that are called out in the corresponding Downtown Pedestrian System map above. Pedestrian System Types I through VI are grouped together in a common table as they are located along street fronts.



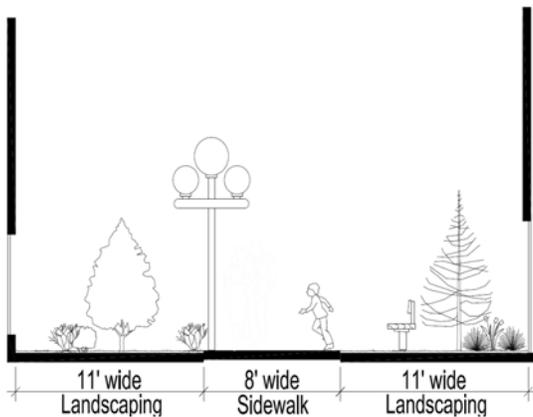
Cross Section	Ped. Zone			Street	Ped. Zone		
	Front Yard	Sidewalk	5' Planting Strip or 4' Furniture Zone with Tree Grates		5' Planting Strip or 4' Furniture Zone with Tree Grates	Sidewalk	Front Yard
I	0	10	4		4	10	0
II	0	10	4		4	10	0
III	14	6	5		5	6	14
IV	7	8	5		5	8	8
V	17	8	5		5	8	17
VI	8	8	4		4	4	8



Type VII – A 30-foot-wide combination walkway/vehicular lane.



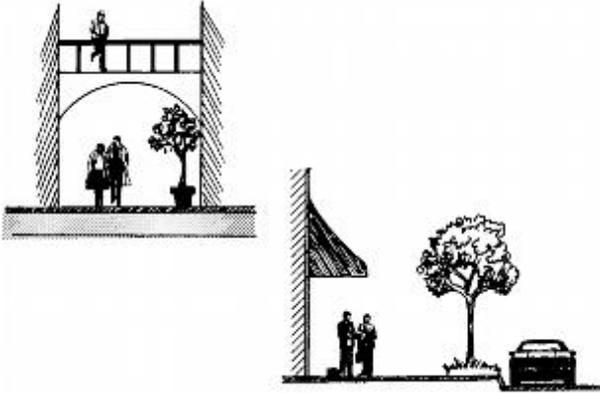
Type VIII – A 12 foot asphalt trail following natural elements such as rivers and streams, with pedestrian connection to buildings.



Type IX - A 30-foot-wide mid-block pathway with an 8-foot sidewalk in the middle and 11 feet of landscaping/plaza on each side.

INSERT MAP 10.4 TOWN CENTER PEDESTRIAN

- (D) Easements/Dedications. Where a pedestrian system walkway exists or is required outside of a public right-of-way, an easement or the dedication to the City of Redmond may be required to provide continuity of the walkway to adjoining property. In case of dedication, residential density shall be calculated based on pre-dedication lot area.
- (E) Permitted Encroachments. Upper floors of buildings, marquees, potted plants, awnings, blade signs, and roof projections may extend over the pedestrian system when the encroachment is integrated into the pedestrian system by providing a covered walkway, plaza, or it otherwise complements pedestrian activities.



Buildings, marquees and roof projections may extend over pedestrian systems when the encroachment is integrated into the pedestrian system by providing a covered walkway, plaza, or it otherwise complements pedestrian activities.

- (F) Width Measured from Back of Curb. Where a pedestrian system adjoins a public street, the system's width shall be measured from the back of the existing or proposed curb.
- (G) Construction Standards. Construction standards for sidewalks are identified in the City of Redmond's Standard Specifications and Details.
- (H) Driveway Crossings. Driveways crossing the pedestrian system shall be minimized and joint use of driveways required, when feasible, to separate vehicles and pedestrians. Areas in driveways will not be calculated as part of the area required to be landscaped in the pedestrian system.
- (I) Access to Buildings. Pedestrian access from the primary building to the pedestrian system along the street shall not be interrupted by vehicular circulation, parking, or other elements that discourage pedestrian use.
- (J) Interior Block Pedestrian System. Interruptions of mid-block pedestrian systems by vehicular circulation or parking are not permitted.
- (K) Variations Not Meeting Standards. Variations in the pedestrian system that do not meet minimum standards may be approved by the Technical Committee. Variations may be allowed after consideration of the following factors:
- (1) Existing right-of-way available to meet standards;
 - (2) Existing buildings encroaching in linkage area;
 - (3) Pedestrian and vehicular volumes anticipated;
 - (4) Existing vegetation;
 - (5) Disruption of system continuity;
 - (6) Accessibility to buildings.

(L) Street Trees Generally. Street trees within the Downtown neighborhood shall be provided as noted on the map entitled "Downtown Street Tree Plan" which is on file in the office of the Planning Department. As property is developed or redeveloped, trees shall be installed or otherwise provided for by the property owner/developer. For streets which do not list tree types or spacing requirements, refer to the City of Redmond Street Tree Plan.

(M) Requirements for Street Trees.

(1) Location. Trees shall be spaced on average as noted on the Downtown Street Tree Plan. Trees shall be planted in planter strips where they exist or are required per this Division 30. Where sidewalks are required to be contiguous with street curbs, trees shall be planted in irrigated tree wells, with City-approved root barriers, next to the street. Street trees may be grouped in larger planters near the curb, if found more appropriate through the Administrative Design Flexibility process. Street trees that cannot be placed next to the street due to inadequate planter strip width, street furniture, driveways, or utilities shall be planted in the abutting yard area.

(2) Street trees shall be planted according to guidelines outlined in RZC 21.32 Landscaping.

(N) Downtown Street Cross Sections.

(1) Guidelines for Application.

(a) The Technical Committee shall review and approve each component of the street cross section on a project by project basis and has the authority to alter street cross section widths and uses.

(b) Street Cross Section widths apply at the middle of the block.

(i) The widths and existence of each component may vary at intersections, as determined by the Technical Committee.

(ii) Intersection design shall be based upon the Pedestrian Program Plan and Bicycle System Plan chapters of the TMP, the Bicycle Facilities Design Manual, the City's Construction Specifications (RZC Appendix 2), and any corridor study adopted by the City Council for the street(s) in question.

(c) Dedicated Right Of Way shall be 60 feet, except in cases where there is more than one General Purpose lane going the same direction, wherein the dedicated Right-of-Way shall be determined by the Technical Committee. Any sidewalk width required by the Downtown Pedestrian System Map exceeding the required Right-of-Way shall be provided through an easement.

(d) Provisions of medians and left-turn lane access shall be determined on a project-by-project basis, based on traffic speeds, volumes and collision history and using recognized engineering standards, such as those published by AASHTO, ITE, or other recognized authority.

(e) Utilities, such as power, telephone and cable, shall be placed under the sidewalk.

(f) When designing multimodal corridors refer to the Modal Integration section of the Transportation Master Plan. Corridors shall support all modes.

(g) See Required Public Improvements (RZC 21.52.030(F)) to review additional options and requirements.

(O) Downtown Streets Cross Sections.

Downtown Streets Cross Sections											
			Southbound/Westbound				Northbound/Eastbound				
			Street								
Street	From	To	On-Street Parking Width	Bike Lane Width	General Purpose Lanes Width	Median / Two Way Left Turn Lane Width	General Purpose Lanes Width	Bike Lanes Width	On-Street Parking Width	Mid-Block Right-of-Way Width	Curb-to-Curb Width
158th Ave NE	NE 85th St	NE 83rd St	8	0	14	0	14	0	8	60	44
158th Ave NE	NE 83rd St	Redmond Way	18	0	11	0	11	0	8	60	48
159th PI NE	Bear Creek Parkway	Leary Way	8	0	11	0	11	0	8	60	38
160th Ave NE	NE 90th St	NE 85th St	8	0	12	12	12	0	0	60	44
160th Ave NE	NE 85th St	NE 83rd St	8	0	14	0	14	0	8	60	44
160th Ave NE	NE 83rd St	Redmond Way	0	0	11	11	11	0	0	60	33
161st Ave NE	NE 90th St	NE 87th St	0	5.5	11	11	11	5.5	0	60	44
161st Ave NE	NE 87th St	Redmond Way	0	6	12	12	12	6	8	60	56
161st Ave NE	Redmond Way	Bear Creek Parkway	8	5.5	12	0	12	5.5	8	60	51
164th Ave NE/Red-Wood Rd	NE 90th St	NE 80th St	0	5.5	11	11	11	5.5	0	60	44
164th Ave NE	NE 80th St	Redmond Way	0	0	12	12	12	0	0	60	36
164th Ave NE	Redmond Way	NE 76th St	8	0	12	0	12	0	8	60	40
165th Ave NE	NE 85th St	NE 80th St	8	0	11	0	11	0	8	60	38
166th Ave NE	NE 85th St	NE 76th St	0	5.5	11	11	11	5.5	0	60	44

Downtown Streets Cross Sections													
			Southbound/Westbound				Northbound/Eastbound						
			Street										
Street	From	To	On-Street Parking Width	Bike Lane Width	General Purpose Lanes Width	Median / Two Way Left Turn Lane Width	General Purpose Lanes Width	Bike Lanes Width	On-Street Parking Width	Mid-Block Right-of-Way Width	Curb-to-Curb Width		
167th Ave NE	NE 85th St	NE 83rd St	0	0	11	0	11	0	8	60	30		
168th Ave NE	NE 80th St	Redmond Way	8	0	14	0	14	0	8	60	44		
169th Ave NE	NE 82nd St	NE 79th St	8	0	12	0	12	0	8	60	40		
170th Ave NE	NE 80th St	NE 79th St	8	0	11	0	11	0	8	60	38		
170th PI NE	NE 80th St	Avondale Way	8	0	11	0	11	0	8	60	38		
170th PI NE	Avondale Way	Redmond Way	0	0	11	12	11	0	0	60	34		
Leary Way	NE 80th St	Cleveland St	8	0	12	0	12	0	8	60	40		
Leary Way	Cleveland St	BNSF	0	0	11	11	11	0	0	60	33		
Leary Way	BNSF	Bear Creek Parkway	0	0	12	12	24 (1)	0	0	78	48		
Leary Way	Bear Creek Parkway	Sammamish River	0	0	24 (1)	0	24 (1)	0	0	72	48		
Avondale Way	NE 80th St	Redmond Way	0	5.5	11	11	22 (1)	5.5	0	70	55		
Avondale Way	Redmond Way	NE 76th St	0	0	11	12	11	0	0	60	34		
NE 79th St	Redmond Way	168th Ave NE	8	0	12	0	12	0	8	60	40		
NE 79th St	168th Ave NE	Avondale Way	8	0	13	0	13	0	8	60	42		
NE 80th St	Redmond Way	Leary Way	8	0	14	0	14	0	8	60	44		

Downtown Streets Cross Sections													
			Southbound/Westbound				Northbound/Eastbound						
			Street										
Street	From	To	On-Street Parking Width	Bike Lane Width	General Purpose Lanes Width	Median / Two Way Left Turn Lane Width	General Purpose Lanes Width	Bike Lanes Width	On-Street Parking Width	Mid-Block Right-of-Way Width	Curb-to-Curb Width		
NE 80th St	Leary Way	164th Ave NE	0	0	20	12	12	0	0	60	44		
NE 80th St	164th Ave NE	170th PI NE	8	0	14	0	14	0	8	60	44		
NE 83rd St	158th Ave NE	160th Ave NE	8	0	11	0	11	0	20	60	50		
NE 83rd St	160th Ave NE	161st Ave NE	20	0	11	0	11	0	8	60	50		
NE 83rd St	161st Ave NE	490 feet east of center of 161st Ave NE and NE 83rd Street intersection	12	0	14	0	14	0	12	60	52		
NE 83rd St/Transit Center	490 feet east of center of 161st Ave NE and NE 83rd Street intersection	164th Ave NE	0	0	15	12	15	0	0	60	42		
NE 83rd St	164th Ave NE	166th Ave NE	8	0	14	0	14	0	8	60	44		
NE 83rd St	166th Ave NE	167th Ave NE	8	0	11	0	11	0	0	60	30		
NE 85th St	Sammamish River	164th Ave NE	7	5	10.5	11	10.5	5	7	60	56		
NE 85th St	164th Ave NE	166th Ave NE	0	5	11	12	11	5	0	60	44		
NE 85th St	166th Ave NE	167th Ave NE	8	0	11	0	11	0	0	60	30		
NE 87th St	161st Ave NE	164th Ave NE	8	0	16	0	16	0	8	60	48		

Downtown Streets Cross Sections													
			Southbound/Westbound				Northbound/Eastbound						
			Street										
Street	From	To	On-Street Parking Width	Bike Lane Width	General Purpose Lanes Width	Median / Two Way Left Turn Lane Width	General Purpose Lanes Width	Bike Lanes Width	On-Street Parking Width	Mid-Block Right-of-Way Width	Curb-to-Curb Width		
NE 90th St	Sammamish River	161st Ave NE	0	5.5	22 (1)	0	22 (1)	5.5	0	82	55		
NE 90th St	161st Ave NE	164th Ave NE/ Red-Wood Rd	0	6	12	12	12	6	0	60	48		
Redmond Way	Sammamish River	160th Ave NE	0	0	24 (1)	12	24 (1)	0	0	102	60		
Redmond Way	160th Ave NE	168 th Ave NE	10	0	11	11	11	0	10	60	53		
Redmond Way	168th Ave NE	NE 76 th St	0	0	26 (1)	24 (2)	26 (1)	0	0	100	76		
Cleveland St	Redmond Way	Redmond Way	8	0	11	0	11	0	8	60	38		
Bear Creek Parkway	Redmond Way	Leary Way	8	0	11	12	11	0	8	60	42		
Bear Creek Parkway	Leary Way	Bear Creek Parkway/170th Ave NE	0	0	11	12	11	0	0	60	34		
Bear Creek Parkway	Bear Creek Parkway/170th Ave NE	Redmond Way	0	0	24	12	24	0	0	85	60		

(1) Width is taken up by two General Purpose lanes.

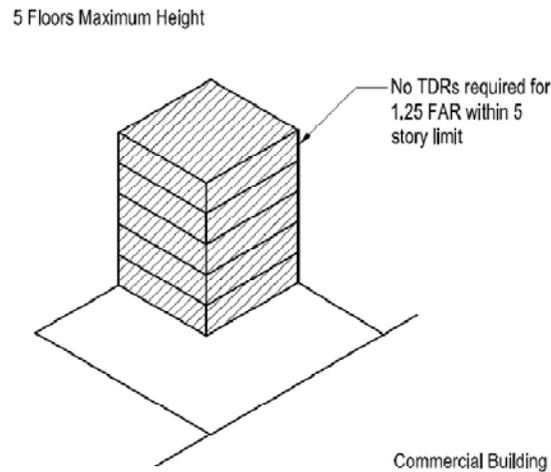
(2) Width is taken up by two turn lanes. Width may define business access and/or right-turn lanes.

21.10.160 Using Transfer of Development Rights (TDRs)

(A) Maximum Floor Area Ratio and Use of TDRs.

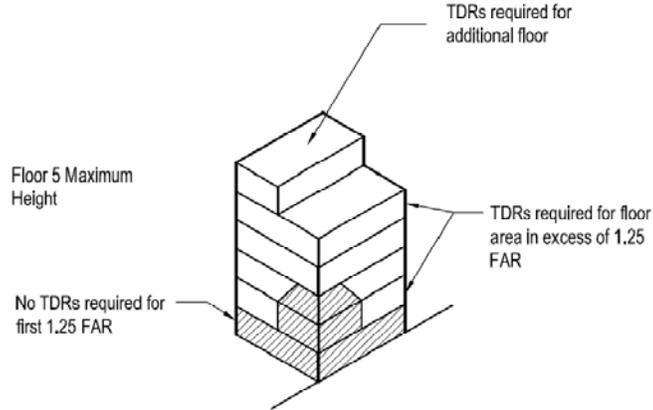
- (1) This section, in conjunction with the Allowed Uses and Basic Development Standards Tables in RZC 21.10.030 through RZC 21.10.100, above, sets a maximum floor area ratio (FAR) for all Downtown developments, unless additional floor area is transferred to the development through the TDR program authorized by RZC 21.48, Transfer of Development Rights Program [or through compliance with the Green Building Infrastructure Program, RZC 21.67](#). The maximum FAR applies to the gross floor area of all buildings on the site, excluding parking areas for motor vehicles that are not maintained as stock in trade. Requirements and examples are set forth in the following subsections.
- (2) A maximum FAR of 1.25 is the maximum development that can be achieved for nonresidential uses without purchasing TDRs [or compliance with the Green Building Program](#). All buildings shall be allowed a minimum of 10,000 square feet of gross floor area regardless of this maximum FAR. In Example 1 below, a five-story building with a 1.25 FAR is shown.

EXAMPLE 1



- (3) Additional FAR for commercial uses can be achieved by purchasing TDRs [or complying with a FAR bonus under the GBP](#) for the commercial floor area in excess of 1.25 FAR. Buildings may also add one floor beyond the five-story maximum building height by purchasing TDRs for height [or meeting the height bonus requirements under the GBP](#). Buildings may not exceed the maximum height limit in any height limit overlay established by the Zone Regulations, Division 20. In Example 2, the 1.25 FAR that can be achieved for commercial uses without TDRs [or GBP](#) is shaded, and the area that is not shaded shows both the additional commercial FAR that can be achieved with TDRs [or the GBP](#) within the five-story height limit and the additional sixth story that may be achieved with the purchase of TDRs [for height or complying with the height bonus provision under the GBP](#).

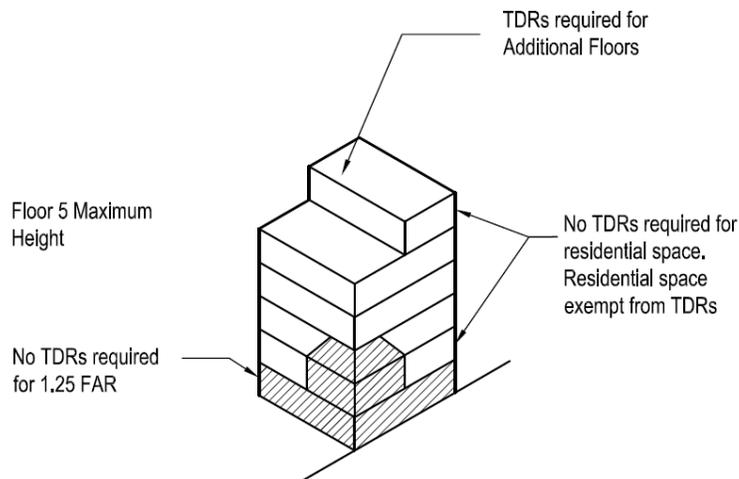
EXAMPLE 2



Commercial Building

- (4) The purchase of TDRs [or compliance with the GBP](#) is not required for residential floor area within the maximum building height. In Example 3 below, the area that is not shaded represents residential floor area in a mixed use building. The shaded area represents the first 1.25 FAR that is exempt from the requirement to purchase TDRs [or compliance with the GBP](#). Where five stories is the maximum allowed height, a sixth story may be added with the purchase of TDRs [or complying with the GBP](#) for the additional floor area (one additional floor only), except where a height limit overlay would be established in the Allowed Uses and Basic Development Standards Tables in RZC 21.10.030 through RZC 21.10.100, and RZC 21.10.110(B), Height Limit Overlay, would prohibit a sixth floor.

EXAMPLE 3



Mixed-Use Residential Building

21.10.170 References

For information on how to measure various site requirements like height and setbacks, see RZC 21.16 Site Requirements

21.20, Affordable Housing

Article II, Design Standards

21.32, Landscaping

21.34, Lighting

21.38, Outdoor Storage, Retail Display and Garbage and Recycling Enclosures

21.40, Parking

[21.67 Green Infrastructure Incentive Program](#)

Article V, Land Division Regulations

Interjurisdictional Review